

# CULHAM NEIGHBOURHOOD PLAN

2020 – 2041



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## SUBMISSION PLAN

Published by Culham Parish Council for Submission under the Neighbourhood Planning (General) Regulations 2012 (as amended).

May 2022

## A Guide to Reading this Plan

Of necessity, this Neighbourhood Plan is a detailed technical document. The purpose of this page is to explain the structure and help you find your way around the plan.

### 1. Introduction & Background

This section explains the background to this Neighbourhood Plan and how you can take part in and respond to the consultation.

### 2. The Neighbourhood Area

This section details many of the features of the designated area.

### 3. Planning Policy Context

This rather technical section relates this Plan to the National Planning Policy Framework and the planning policies of South Oxfordshire District Council and the emerging Oxfordshire Plan 2050.

### 4. Community Views on Planning Issues

This section explains the community involvement that has taken place.

### 5. Vision, Objectives & Land Use Policies

This is the key section. Firstly, it provides a statement on the Neighbourhood Plan Vision and Objectives. It then details Policies which are proposed to address the issues outlined in the Foreword and in Section 4. These Policies are listed on page 5. There are Policy Maps at the back of the plan and additional information in the Appendices to which the policies cross reference.

### 6. Implementation

This section explains how the Plan will be implemented and future development guided and managed. It suggests projects which might be supported by the Community Infrastructure Levy which the Parish Council will have some influence over. Finally, it deals with a number of issues which although relevant are outside the scope of a Neighbourhood Plan.

# CULHAM NEIGHBOURHOOD PLAN

2020 – 2041

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## FOREWORD

The submitted Plan represents the views of the local community with regard to the future of Culham up to 2041. It has been prepared by the Parish Council and with support from the local people, statutory bodies and our advisors. In our view, it represents a strong and robust approach to the Localism agenda. Within this context it has been designed to operate within the adopted South Oxfordshire Local Plan in the Culham Neighbourhood Plan area which will impact dramatically on our parish.

### Our Ambitions

The Plan has been designed to be a planning, land use document. However, within this important context it delivers several overlapping community ambitions in Culham as follows:

- A wish to safeguard the rural character of the village.
- A wish to enhance community infrastructure.
- A wish to enhance and develop green spaces, wildlife and biodiversity.
- A wish to promote sustainable development in the form of carbon neutral building.

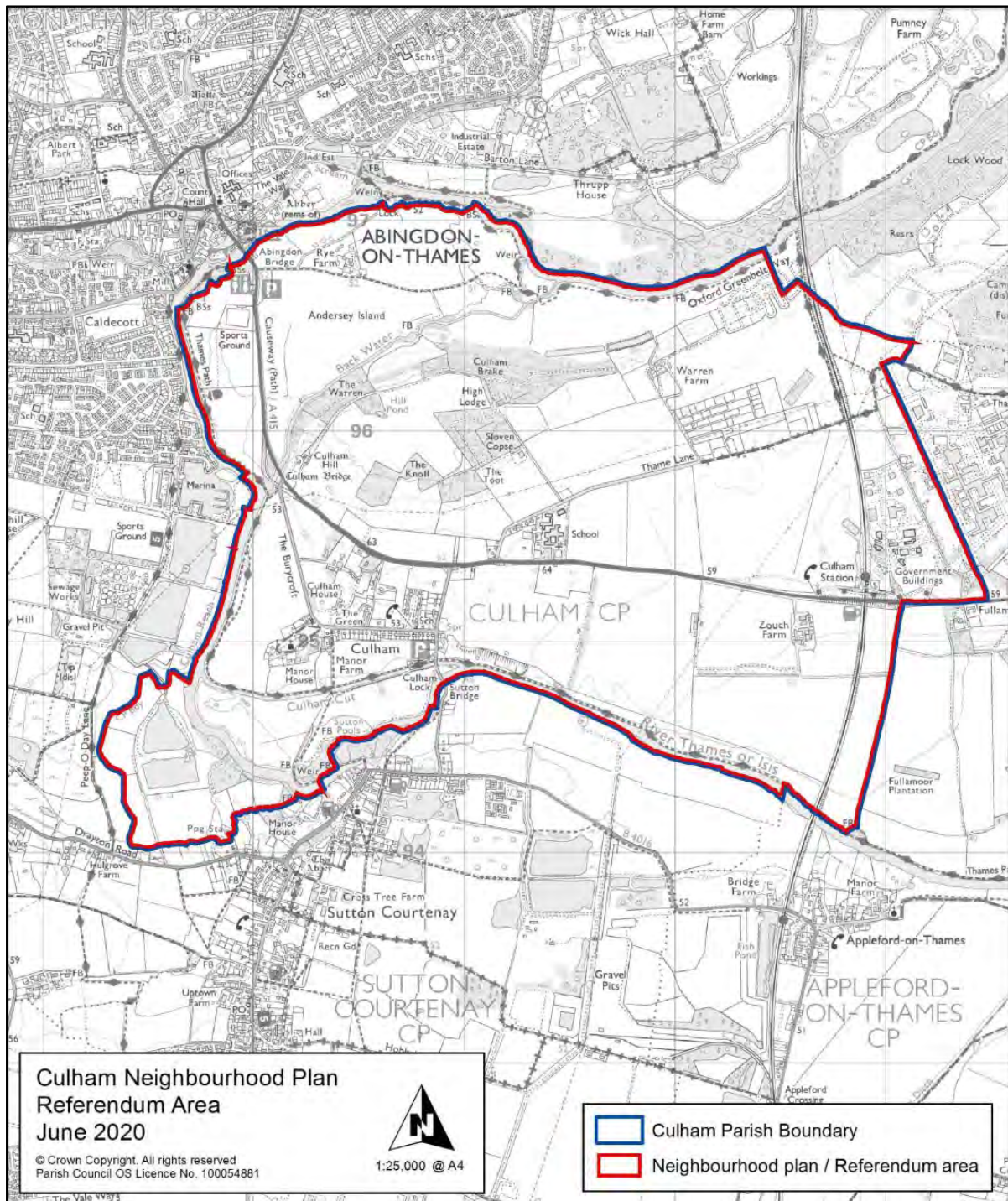
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# 1. INTRODUCTION & BACKGROUND

1.1 Culham Parish Council is preparing a Neighbourhood Plan for the area designated by the local planning authority, South Oxfordshire District Council, on 18 September 2020. The plan is being prepared in accordance with the Neighbourhood Planning (General) Regulations of 2012 (as amended).

1.2 The area coincides with the parish boundary (see Plan A below).



Plan A: Designated Neighbourhood Area

1.3 The purpose of the Neighbourhood Plan is to set out a series of planning policies that will be used to determine planning applications in the area in the period to 2041. The Plan will form part of the development plan for the district, alongside the South Oxfordshire Local Plan 2035. There is also an emerging Oxfordshire Plan 2050 and the emerging Joint Local Plan 2041, the latter covers the same plan period as the Neighbourhood Plan.

1.4 Neighbourhood Plans provide local communities, like Culham, with the chance to manage the quality of development of their areas. Once approved at a referendum, the Plan becomes a statutory part of the development plan for the area and will carry significant weight in how planning applications are decided. Planning applications are decided in accordance with the development plan, unless material considerations indicate otherwise. Plans must therefore contain only land use planning policies that can be used for this purpose. This often means that there are important issues of interest to the local community that cannot be addressed in a Plan if they are not directly related to planning.

1.5 Although there is considerable scope for the local community to decide on its planning policies, Neighbourhood Plans must meet some 'basic conditions'. In essence, the conditions are:

- Does the Plan have regard to national planning policy and guidance?
- Is the Plan in general conformity with the strategic policies contained in the development plan?
- Does the plan contribute to the achievement of sustainable development?
- Has the process of making the plan met the requirements of environmental law?

1.6 In addition, the Parish Council will need to demonstrate to an independent examiner that it has successfully engaged with the local community in preparing the Plan. Following an independent examination, South Oxfordshire District Council will determine if they should accept the examiner's **recommendation/s**, if the Plan meets the basic conditions, and if the Plan should proceed to a referendum of the local electorate. If a simple majority of the turnout votes in favour of the Plan, then it becomes adopted as formal planning policy for the Parish.

#### The Levelling Up White Paper

1.7 In February 2022 the Government published for consultation its White Paper, '[Levelling Up the United Kingdom](#)', which proposes to make changes to planning system. It indicates that there is still a future for neighbourhood planning in that system. It remains unknown when any proposed changes will be implemented.

#### The Pre-Submission Plan

1.8 A draft Pre-Submission Plan was published for consultation 17 January – 14 March 2022 in line with the Regulations. The Parish Council has reviewed the comments received from the local community and other interested parties and has made changes to this final version. They have also updated some of the reports included in the appendices of the plan.

## Sustainability Appraisal & the Habitats Regulations

1.9 The District Council has confirmed in its screening opinion that the proposals of the Neighbourhood Plan do not have the potential for significant environmental effects and therefore no Strategic Environmental Assessment will be necessary as per the Environmental Assessment of Plans & Programmes Regulations 2004.

1.10 The screening opinion also confirms that, given the plan scope and the absence of any Natura 2000 sites in the designated Neighbourhood Area, a Habitats Regulations Assessment will not be required, as per the Conservation of Habitats and Species Regulations 2017 (as amended).

## 2. THE NEIGHBOURHOOD AREA

*(Based on the Culham Parish Council and the village run website about Culham and its history)*

2.1 Culham is a small village and civil parish in a bend of the River Thames, 1 mile (1.6 km) south of Abingdon in Oxfordshire with more than 12 centuries of recorded history. The 2011 Census recorded the population of Culham as 453. The Parish lies entirely within the Green Belt (and the village itself is 'washed over').



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2.2 Though small, it is known internationally through the nearby research centre, Culham Science Centre, home to two major nuclear fusion experiments, JET and MAST. When JET was built, the European Commission set up the European School in Culham to provide an international education for the children of Euratom scientists who would come to work at JET. The European Commission withdrew from managing the school, and it is now a multicultural multilingual UK state school called the Europa School UK.



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2.3 The parish of Culham divides geographically into three distinct sections. Most of it lies between Clifton Hampden and a backwater of the Thames once known as Swift Ditch: Andersey Island, comprising the area between the backwater and Abingdon; and the Otneys, an area on the right bank of the Thames adjoining the west side of Sutton Courtenay.

2.4 The origins of the parish system go back to Anglo-Saxon times. We do not know when the parish of Culham first came into existence, but a survey of it was made in 940 in the time of King Edmund. The boundaries of the parish seem to be exactly as now, except for the loss of some eyots in the river to Abingdon in 1894. The survey mentions the ford where Abingdon Bridge now stands and refers to 'barrows' (earthworks) at some points along the Parish's eastern boundary; but all trace of the barrows has long since disappeared.

2.5 The parish is bounded by the Thames to the north, west and south, and by present and former field boundaries to the east. It is low-lying and fairly flat, rising from the Thames floodplain in the south to a north-facing escarpment in the north up to 260 feet (80 m) above sea level. The Thames was certainly navigable during the Middle Ages from London to Henley, and perhaps to Burcot; but the barges moving upstream from Burcot had to face a shallow, rocky bottom at Clifton and a very tricky passage through Sutton to Abingdon. There was, of course, no Clifton or Culham Cut until the 19<sup>th</sup> Century. At Abingdon the river was again shallow and there were numerous obstructions on the way to Oxford. Hence the wharfage for Abingdon came to be at Culham. We know, for instance, that stone and lead from the dissolved Abbey of Abingdon were brought by road to Culham Wharf to be loaded upon barges for transportation to London. In Tudor times barges became bigger and this made it almost impossible for them to move between Burcot and Oxford. Hence by two Acts of 1605 and 1624 Parliament set up the Oxford - Burcot Commission to improve the passage of the Thames between these places.



© unused RP postcard – Photo: Piper, Didcot Source: Sutton Courtenay Local History Society

2.6 The Commission did much to improve the river between 1624 and the outbreak of the Civil War in 1642. It built the first pound locks on the Thames at Iffley, Sandford and Culham. The Culham Lock was constructed about 1636 in a new cutting at the head of Swift Ditch, which was made the main artery for the barge traffic. The remains of the lock can still be seen as well as the assembly pool for barges that lay near it. There was a flash lock about half way along Swift Ditch, which existed at least as early as 1585. Swift Ditch remained the chief navigation channel until 1790 when it was abandoned in favour of the present route through Abingdon. Water communications through Culham were made much easier in 1809 with the construction of The Culham Cut and Lock. The Cut was made partly along the line of the old Speel Ditch, a straggling channel that left the Thames at the head of the present Cut and turned south to rejoin the river near Sutton Mill.

2.7 Communications by road were poor until the early 15<sup>th</sup> Century. The main Dorchester - Abingdon road runs through the parish from east to west, but before the reign of Henry V the traveller from Dorchester had to ford the river both at Culham and Abingdon. The highway from Dorchester to Abingdon is undoubtedly very old - it is said in an Act of Parliament of 1416 to have existed from "time immemorial". Between 1416 and 1422 a major scheme for improving communications between Abingdon and Culham was undertaken by the Abingdon Guild of the Holy Cross. Abingdon Bridge, the causeway across Andersey, and the old bridge at Culham were built at the Guild's expense.



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2.8 The old bridge is built across the site of the ancient ford known as Culham Hyth; it is of stone and has five perpendicular arches. It lies just to the south of the new bridge erected in 1928 by the Oxfordshire County Council. An Act of Parliament in 1736 created a turnpike trust to maintain the roads between Henley and Abingdon; the trust was empowered to levy tolls for the repair of the roads. Not until 1875 were tolls completely abandoned. The trust set up toll-houses at Culham Bridge and at the junction of Thame Lane with the main highway. The toll-houses are still standing.

2.9 During the English Civil War the bridge had considerable strategic importance. After the Royalists left Abingdon in May 1644 the Parliamentarians seized Culham Bridge and harried the royalist food convoys on the way to Oxford. The Royalists tried to recapture the bridge and demolish it in January 1645. This resulted in a skirmish on 11 January known as the battle of Culham Bridge. Sir Henry Gage "while boldly leading his men in a third assault on the enemy... was hit by a bullet and killed".

2.9 The highway is joined near the Wagon and Horses Inn by Thame Lane, which used to continue its journey across Clifton Heath. It was cut in 1941 when a Royal Naval Air Station was built on the east side of the railway line between Didcot and Oxford. A field to the north of Thame Lane bounded by the railway line was probably the site of the Abingdon races, held on Culham Heath from the 1730's to 1811. Visitors from Oxford could approach the racecourse by a road, or rather track, from Nuneham.

2.10 Culham village was never on the main road. The village High Street is part of a long loop beginning at the Wagon and Horses and ending at Culham Bridge. Before 1813 the straight stretch of road from Culham Bridge to the Village green, cutting through Bury Croft, did not exist; the main highway was linked to the village by a road running close to the west side of Culham House. This road was closed when the straight stretch of road to the Bridge was made. Before 1807 a road from the Wagon and Horses ran to the ferry which took travellers over the Thames to Sutton. The ferry lay just to the west of the present bridge. Built in 1807, it was extended over the Culham Cut in 1908. It was privately owned until 1939 when it was jointly purchased by the Berkshire and Oxfordshire County Councils.

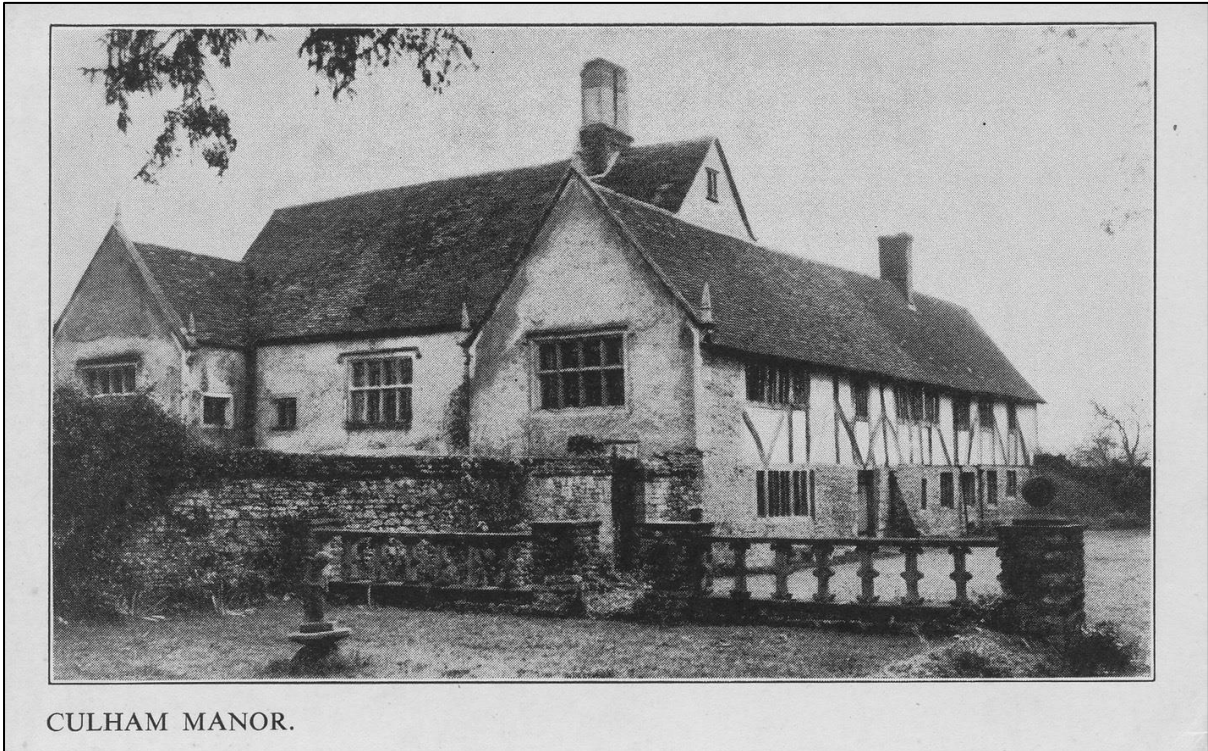


© British Listed Buildings

2.11 The railway line from Didcot to Oxford runs through the eastern fringe of the parish. It was built in 1843 and 1844 after the objections of local landowners, the University and the city of Oxford had been overcome. The local station was known as "Abingdon Road" and was served by horse-drawn omnibuses from Abingdon which were timed to meet the trains. When Abingdon secured its own station in 1856 "Abingdon Road" was rechristened "Culham". The old ticket office at Culham Station, a Grade II\* listed building, was designed by the famous Victorian engineer Isambard Kingdom Brunel.

2.12 Culham's old English name (Cula's Hamm) suggests a possible 6<sup>th</sup> Century Anglo-Saxon settlement in the bend of the river, and it was a place of some importance in later Saxon times. For six centuries it was a possession of the Abbey of Abingdon, though the Abbey did not have continuous possession before the middle of the 10<sup>th</sup> Century; and it was 150 years after that before the Abbey finally secured Andersey. The Mercian King Offa (d.796) is said to have had a hunting lodge on Andersey. The remainder of the parish was apparently in royal hands at this time. The abbey later claimed that King Kenwulf of Mercia (796-821) had granted Culham to it

and produced two charters, dated 811 and 821 to prove its case. The charters are certainly spurious, but may nonetheless have a basis of truth. The forgery of documents by monks was a not unusual procedure in the Dark Ages; they probably forged them to ensure their Abbey's possessions had a legal basis. This may well be the case with Culham. Certainly, Culham enjoyed a spell of royal favour in the Middle Ages.



© unused postcard photo, Source: Sutton Courtenay Local History Society

2.13 The manor of Culham remained in the hands of Abingdon Abbey as a rest house until the dissolution of the Abbey in 1538 when it was seized by the Crown. In 1545 Henry VIII granted it to a London wool merchant, William Bury, in exchange for land in the Isle of Sheppey and £600. The house is largely of fifteenth century origin but in 1610 Thomas Bury rebuilt the north front. Bury's house was much larger than the present one, for an eastern section was demolished during the Civil War. The Manor House was in possession of the Bisshopp family from 1666 until 1856 but their interest in it ceased in 1749 and the Manor began a long period of decline; for many years it was a farm house. However, the house was restored splendidly by Sir Esmond Over from its sadly dilapidated state of 1933.

2.14 The majority of villagers obtained a living from agriculture, farming strips of land in the great open fields which surrounded the village. Originally, there were two vast arable fields, perhaps even as late as 1539; for a survey of that year speaks only of Town and Contard Fields. By the middle of the 17<sup>th</sup> Century there were three fields (Ham, Middle and Contard); during the 18<sup>th</sup> century there was a change to a four field system. The enclosure Award of 1813 mentions four fields: 1. Contard - forming a triangle between the main highway and Thame Lane and ending in the east at Culham Heath, 2. Ham - south of the main highway, from the Clifton boundary to a point perhaps half way between the Wagon and Horses and the boundary, 3. South Middle Field - the remainder of the arable area south of the main highway, 4. North

Middle Field - mostly north of the main highway between the Wagon and Horses and Culham Bridge, and also north of Thame Lane for a short distance at its western end.

2.15 The exact boundaries of the fields are hard to trace. They comprised altogether some 700 acres. Apart from the arable land there were before enclosure a number of hedged meadows and pastures: these were to be found on Andersey, on both sides of Swift Ditch and in the north of the parish. In addition, Culham Heath was a large tract of land in the north east of the parish south of Nuneham Park and reaching in places the main Abingdon-Dorchester Road. After enclosure much of the heath was drained and brought under cultivation. The 18<sup>th</sup> century saw the appearance of large farms. Tye, Warren and the Manor Farm were the best known. There were about 40 Houses in the village at this time.



© unused postcard photo - TVAP Oxford Series XXX 1395, Source: Sutton Courtenay Local History Society

2.16 The houses lay mostly north and south of the main village street, i.e. the present High Street, though for most of its length the old street was farther north, i.e. nearer to Culham House, than the present High Street. The alteration to the present line was made between 1810 and 1813 at the time of enclosure when the road across Bury Croft was constructed. Most of the village was rebuilt in 1869 and 1870 and consequently few of the old dwellings survive. Indeed, the only old cottage still in existence is the village store, of 17<sup>th</sup> century origin and refronted in the 18<sup>th</sup> century. Not even the inns can claim much antiquity. The parish now has three: the Wagon and Horses, the Lion and the Jolly Porter (formerly the Railway Hotel). The Wagon and Horses can be traced back to 1795, though the building is early 19<sup>th</sup> century; the Lion (formerly the Sow and Pigs) is a fairly modern building, but it too can be traced back to 1795; the Jolly Porter was built about 1846. In the late 18<sup>th</sup> century there were half a dozen malshouses in the village.

2.17 Culham's oldest Building is the Manor House, originally a medieval grange of the Abbots of Abingdon. The house is largely of 15<sup>th</sup> century date, but in 1610 Thomas Bury rebuilt the north front. Bury's house was much larger than the present one, for an eastern section was demolished during or after the Civil Wars. There is still a room within the house called the Abbot's Chamber which once had heraldic glass depicting the arms of Abbot Coventry, who died in 1512. In the grounds is a dovecote, dated 1685, and bearing the initials of Sir Cecil Bisshopp. It is believed to be one of the three largest in England. When the Bisshopps ceased to bother with Culham, the Manor House began a long period of decline; for many years it was a farmhouse.



**CULHAM HOUSE**

© unused postcard photo, Source: Sutton Courtenay Local History Society

2.18 The largest house in the village is Culham House, built about 1775 by John Phillips, lay rector of the parish. Phillips was a London builder. His ancestors hailed from Hagbourne and became master carpenters to George I and George II. The Phillips family first appeared in Culham about 1736 and were here until 1935. As lay rectors they were entitled to sit in the chancel of the church and were also legally responsible for the chancel's upkeep. Several memorials to members of the family are in the church. John Phillips erected a handsome redbrick building of five bays, with contemporary staircase, overmantles and doorcase. The house was enlarged about 25 years later to seven bays. It was once noted for its collection of china.

2.19 The old Vicarage was built about 1758, probably by Benjamin Kennicott, vicar of Culham 1753-83. It was enlarged by a later vicar, Robert Walker, in 1849. It has now been sold by the church authorities.

2.20 The only large building beyond the confines of the village is Culham College of Education. The building, erected in 1852 was designed by Joseph Clarke, a minor architect of the Victorian era. Clarke designed the College in the neo-Gothic style which was fashionable at the time. The tower block was opened in 1973 when

Teacher Training Colleges were being expanded. Europa School was established in 1912 and took over the school site and pupils fully from the Culham European School in 2017. The European School was located on the site from 1975, in buildings which had previously been the home of Culham College, a Church of England teacher training college from 1853.

2.21 There is no sign of any school in the parish before the early 19<sup>th</sup> century. In 1808 younger children learned to read and write in two small schools, presumably held in cottages; in 1815 a Sunday School was started, its master being paid from the rates. Nevertheless, provision for education was very unsatisfactory until 1850, when the village Church of England School was erected at a cost of £438. Some additions to the premises were made in 1897. Usually a mixed all-age school, it was reorganised in 1924 for infants and girls only, but in 1931 the senior girls were transferred to Dorchester. Temporarily closed in 1948, the school was re-opened in 1951.



© Abingdon-on-Thames Town Council

2.22 St Paul's Church is situated at the end of the village green, opposite the Manor House and gardens. It has a long history and was closely linked in medieval times to Abingdon Abbey. Rebuilt in Victorian times, replacing one of late twelfth century or early thirteenth century origin; the tower is its oldest part, dating back to 1710. The Mediaeval Church was about the same length as the present building but had a narrower nave, In 1852 the mediaeval nave was beyond repair and was rebuilt; the cost was borne partly by a parish rate and partly by donations. The chancel was rebuilt at the expense of the lay rector, John Shawe Phillips. A new parish cemetery bordering the existing churchyard was consecrated by the Bishop of Dorchester in 2004 and the Parochial Church Council received approval from the Home Office for the closure of the churchyard, which is full.

2.23 The parish is also host to the Culham Break Site of Special Scientific Interest (SSSI) on the northern edge of the parish boundary, a small area of wet willow woodland with large sedges and wetland wildflowers on the ground. The site is particularly important for the presence of a large population of the nationally scarce Lodden Lily. Other wetland wildflowers found here include meadowsweet, valerian and yellow flag iris.

2.2 Culham, in the course of its known history of more than 1,000 years, has seen many changes in the evolution of England and many changes within its own boundaries. Yet it remains a unity despite the economic and social pressures of the 20<sup>th</sup> century.



© Aerial View of Culham Village, Source: Culham Campsites

### 3. PLANNING POLICY CONTEXT

3.1 The Parish lies within the South Oxfordshire District Council area in the county of Oxfordshire.

#### National Planning Policy

3.2 The National Planning Policy Framework (NPPF) published by the government is an important guide in the preparation of neighbourhood plans. The following paragraphs of the latest version of the NPPF are considered especially relevant to this neighbourhood plan:

- Neighbourhood Planning (§28 - 30)
- Promoting healthy safe communities (§92)
- Community facilities (§93)
- High quality design (§130)
- Conserving and enhancing the natural environment (§174)
- Conserving and enhancing the historic environment (§190)

3.3 The Government has also set out a requirement for the provision of First Homes in a Written Ministerial statement on 24 May 2021. These requirements were subsequently incorporated into National Planning Practice Guidance. As the Parish is 'washed over' by Green Belt, First Homes Exception Sites are unable to come forward in The Parish. However, this does not preclude First Homes forming part of the affordable housing contributions through Rural Exception Sites allowed for by adopted policies of the South Oxfordshire Local Plan. It is anticipated that the forthcoming Local Plan will deal with this matter appropriately and South Oxfordshire District Council has published an advice note regarding First Homes in the meantime.

#### Strategic Planning Policy

3.4 The Neighbourhood Plan must be in general conformity with the strategic policies of the development plan, which primarily comprises the South Oxfordshire Local Plan (SOLP) 2035 adopted in December 2020. There are also minerals and waste policies adopted by Oxfordshire County Council, but none are considered relevant to the Neighbourhood Plan.

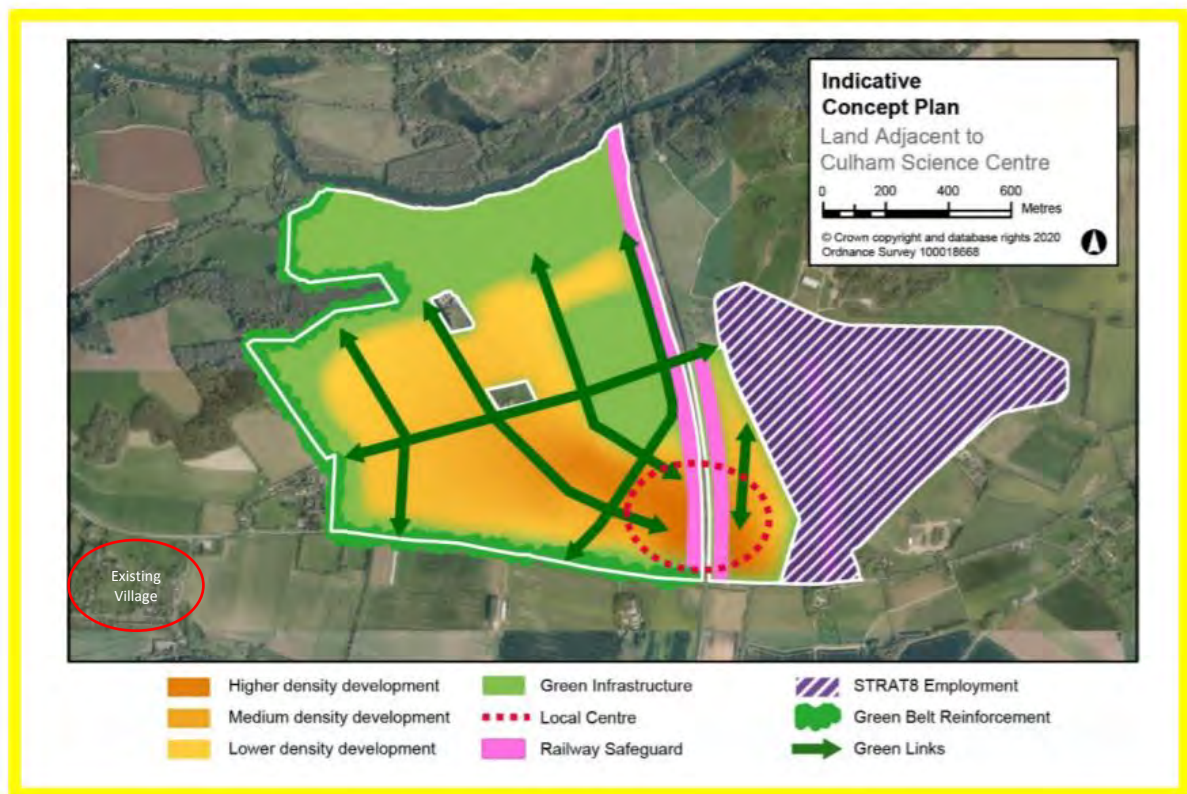
3.5 Culham village is defined as a 'smaller village' in the settlement hierarchy and remains 'washed over' by the Oxford Green Belt. The settlement infill development policy is H16 which allows for infill development and the redevelopment of previously development land or buildings as follows: *"the filling of a small gap in an otherwise continuous built-up frontage or on other sites within settlements where the site is closely surrounded by buildings. The scale of infill should be appropriate to its location."*

3.6 Policy STRAT6 restates the national planning policy approach to development management in the Green Belt. In which case, unless 'very special circumstances' can be demonstrated, proposals will only be supported by this policy if they are deemed appropriate in the Green Belt as set out in NPPF §149 and §150. For housing schemes, that includes proposals for 'limited infilling in villages', which is defined by

Policy H16 and for affordable housing for local community needs as exception sites in line with Policy H10. Policy H8 guides housing development in the Smaller Villages – allowing for growth of up to 10% in the housing stock - but for Culham this policy is curtailed by its location in the Green Belt.

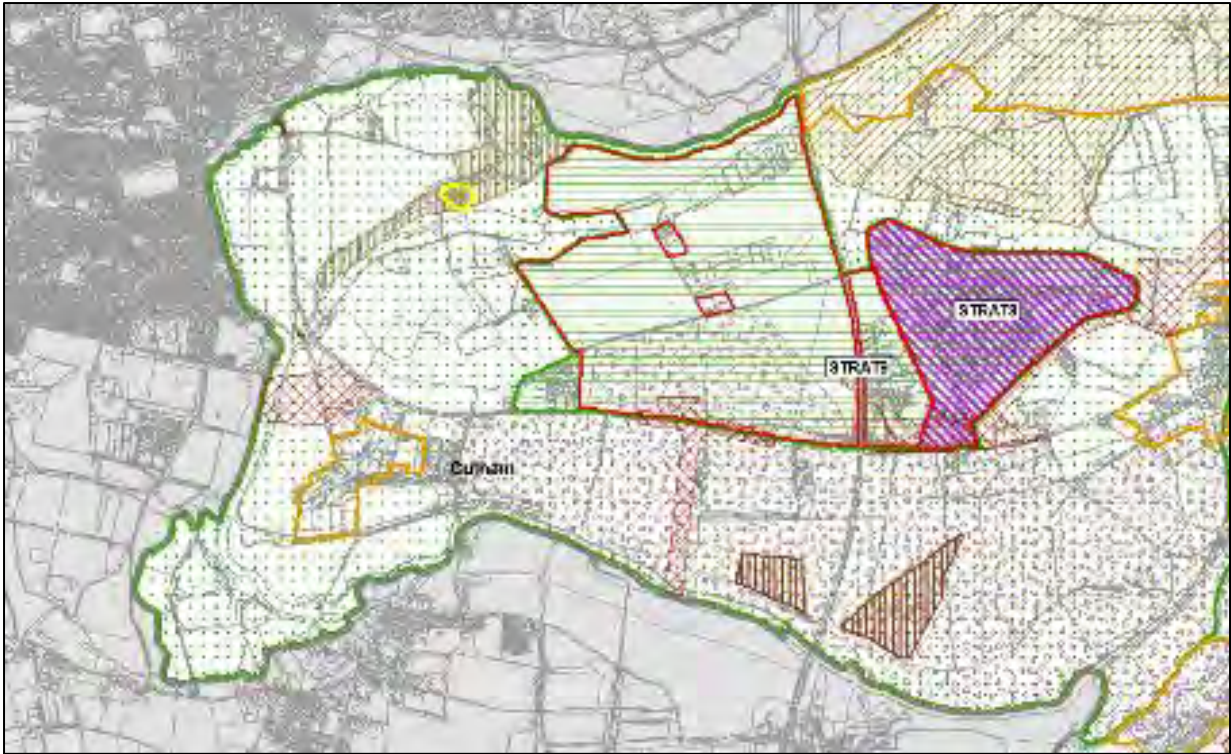
3.7 There are also major strategic allocations and safeguarded land for strategic transport schemes within and adjacent to its boundary as set out below:

- STRAT8: Culham Science Centre – Removing Culham Science Centre from the Green Belt and supporting its redevelopment and intensification to deliver at least a net increase in employment land of 7.3 ha (including the existing 10ha retained but redistributed across the two strategic allocations) in combination with the adjacent strategic allocation at Policy STRAT9 (see Plan B below).
- STRAT9: Land adjacent to Culham Science Centre – Removing land from the Green Belt for a strategic allocation for 3,500 new homes (with 1,850 to be built in the plan period), employment, retail and social infrastructure – including a GP surgery and 2FE primary school focussed on the railway station (see Plan B below).
- TRANS3: Safeguarding land for Strategic Transport Schemes – as shown hatched in red on Plan C overleaf. The policy safeguards land including for the widening of the A4130 at Didcot, Clifton Hampden Bypass, Didcot Science Bridge and Culham River Crossing. These schemes make up the Didcot Garden Town Housing Infrastructure Fund programme (HIF1) and are currently the subject of planning application, R3.0138/21, which at the time of writing is yet to be determined. To manage the highway network a tiered approach to development within the areas affected by HIF1 is currently in operation, enabling some development to come forward prior to the delivery of HIF1.



*Plan B: Extract from South Oxfordshire Local Plan 2035 Indicative Concept Plan showing existing village settlement*

3.8 The existing Culham settlement lies to the south west of STRAT9 and is separated from it by the existing Europa School UK and the A415 (see plan B above). The sheer size and mix of uses proposed at STRAT9 will have an impact on existing services and facilities in the village. Whilst it is possible for Neighbourhood Plans to be used as a vehicle to masterplan strategic sites, in such cases the proposed strategic allocations adjoined existing larger communities. The Culham Neighbourhood Plan has therefore focussed on expressing the identity of the existing village community which may influence future masterplanning of STRAT9 indirectly. The Parish Council continues to engage in the future masterplanning of the site as part of its day-to-day business.



Plan C: Extract from South Oxfordshire Local Plan 2035 Policies Maps November 2020

3.9 There are other policies in the new Local Plan that may be relevant, including:

- ENV1 – Landscape and countryside
- ENV2 – Biodiversity
- ENV4 – Watercourses
- ENV5 – Green Infrastructure
- ENV6 – Historic Environment
- ENV7 – Listed Buildings
- ENV8 - Conservation Areas
- EP4 – Flood Risk as several parts of the village is subject to surface water flooding and much of the area adjacent to the Thames lies within Flood Zones 2 and 3
- DES1 - Delivering high quality development
- DES2 - Enhancing local character
- CF1 - Safeguarding community facilities
- CF4 - Existing open space, Sport and recreational facilities

Oxfordshire Plan 2050

3.10 As part of the Housing and Growth Deal secured by the Oxfordshire Growth Board, the Oxfordshire Plan 2050 is a strategic planning document that will provide a strategic policy framework for the administrative county area of Oxfordshire. A consultation on the scale and broad locations of spatial growth options, among other matters, ran until 8 October with adoption anticipated in May/June 2023. The Parish Council response to the consultation is included in the evidence base.

3.11 The consultation anticipates that the Oxfordshire Plan 2050 will contain a mix of the five spatial options presented:

- Option 1: Focus on opportunities at larger settlements & planned growth locations – the options excludes new significant level growth at villages and new settlements do not form part of this strategy option.
- Option 2: Focus on Oxford-led growth – the option focusses on new or extended urban extension on the edge of the City.
- Option 3: Focus on opportunities in sustainable transport corridors & at strategic transport hubs – the railways line to the eastern fringe of the parish is considered to be included in this option. The consultation anticipates a new national rail strategy from Government to be published shortly which will be taken into account in the next stage of the process.
- Option 4: Focus on strengthening business locations – Culham Science Centre is identified as a business location included in this option.
- Option 5: Focus on supporting rural communities – the option includes extensions to allocations, such as that identified in STRAT 8 & 9 of the SODC Local Plan. It is noted that limited rural growth will feature in the next stage of the process.

3.12 It is clear that the Oxfordshire Plan 2050 will also have an impact on the village. As the Neighbourhood Plan is expected to be examined prior to the adoption of the Oxfordshire Plan 2050, its reasoning and evidence base has been taken into consideration during the preparation of the Neighbourhood Plan, and it will prepare for potential growth in the area through expressing the identity of the existing village community which may influence future masterplanning indirectly. The Parish Council continues to engage in this process.

#### The Joint Local Plan 2041

3.13 South Oxfordshire and Vale of White Horse District Councils are working together to prepare a new Joint Local Plan 2041. Once adopted, the Joint Local Plan 2041 will replace the adopted South Oxfordshire Local Plan 2035. The document is in its early stages of preparation with adoption anticipated to be adopted in October 2024. The Parish Council will engage with the District Councils throughout this process.

#### Neighbourhood Plans

3.14 Neighbourhood planning is popular in the area and a number of plans have been made, or are in preparation, in the vicinity of the Parish, including the made plans in the neighbouring parishes of Drayton & Radley. Sutton Courtenay and Clifton Hampden are in the process of preparing neighbourhood plans. Clifton Hampden are also prepared a Neighbourhood Development Order. With the recent merging of the Culham Parochial Church of England Primary School and Clifton Hampden Church of England Primary School, leading to the formal closure of Culham Parochial Church of England Primary School, the village relies on Clifton Hampden for this service.

3.15 The Clifton Hampden Neighbourhood Development Order proposes to encourage the improvement and extension of the primary school, as well as a new

surgeries. Whilst the provision of new services and facilities, including a new school, at the land allocation in Policy STRAT9 of the SODC Local Plan will have an impact, services and facilities in Clifton Hampden, such as the Clifton Hampden Church of England Primary School currently serve the existing Culham community.

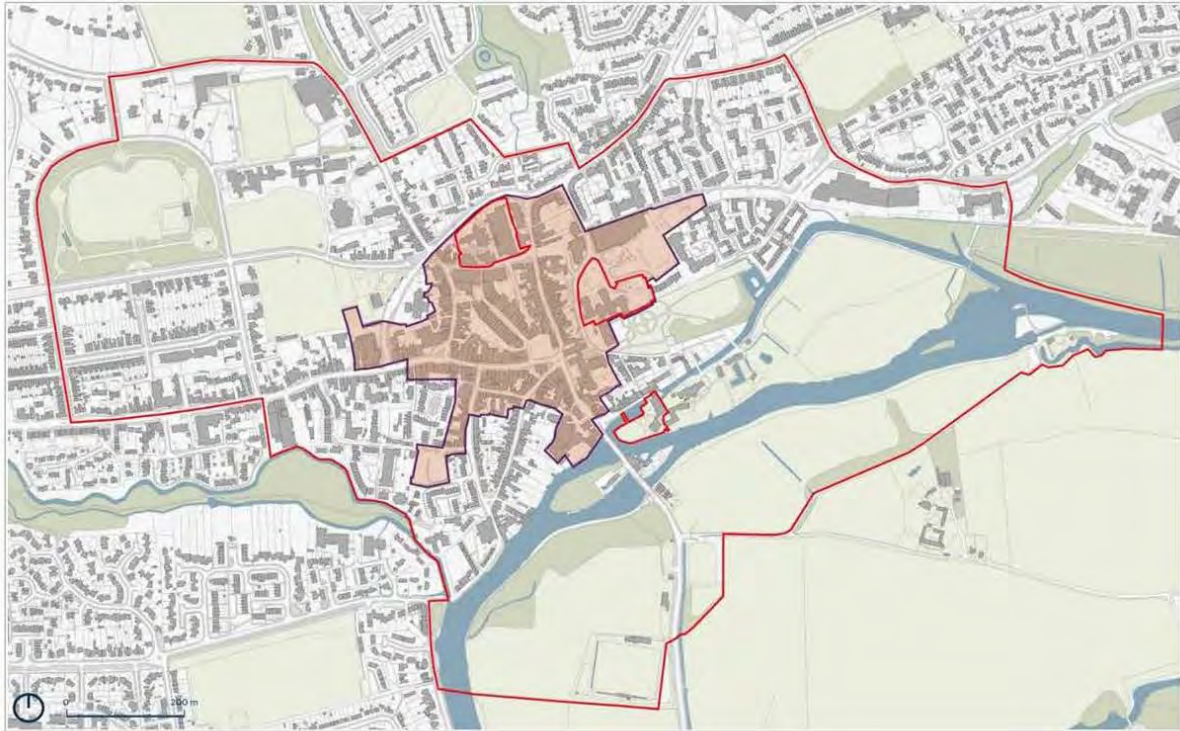
#### Culham Conservation Area

3.16 The Conservation Area was designated by South Oxfordshire District on 11 December 1984 (see orange outline in Plan C). Conservation Areas were introduced by the Civic Amenities Act of 1967, to protect areas of special interest as opposed to individual buildings. Since 1967 some 8,000 conservation areas have been designated in England, including 72 in South Oxfordshire District. Under the Planning (Listed Buildings and Conservation Areas) Act 1990 local authorities have a duty to designate conservation areas and from time to time to review the boundaries. Such areas are defined as 'areas of special architectural or historic interest, the character or appearance of which it is desirable to preserve or enhance'.

3.17 The main attributes that define the special character of an area are its physical appearance and history, i.e. the form and features of buildings and the spaces between them, their former uses and historical development. Where there are a number of periods of historical development, the character of individual parts of the conservation area may differ. Contrasts between the appearance of areas and the combination of buildings of various ages, materials and styles may contribute to its special character.

#### Central Abingdon Regeneration Framework

3.18 The Vale of White Horse District Council (VoWHDC) has established a Central Abingdon Regeneration Framework (CARF) project team. The CARF project is currently in preparation. The aim of the CARF project is to establish a framework for guiding regeneration of central Abingdon, and includes land and property owned by VoWHDC, as well as improvements to other facilities and transport routes. The CARF project study area is shown on Plan D below which includes land in Culham parish. The Parish Council supports the aim of the CARF project and will engage with the CARF project team as part of its day-to-day business (see Section 6 for further detail).



**PROJECT TITLE**  
**VOWHDC**  
**ABINGDON TOWN CENTRE**  
**DRAWING TITLE**  
**ABINGDON TOWN CENTRE**  
**SITE LOCATION PLAN**

**ISSUED BY** London T. 020 7016 0720  
**DATE** March 2022 **DRAWN** EV  
**SCALE** @A3 **1:5000** **CHECKED** JC  
**STATUS** Draft **APPROVED** JC

| REV. | DESCRIPTION | APP. DATE |
|------|-------------|-----------|
|      |             |           |

No dimensions are to be scaled from this drawing.  
 All dimensions are to be checked on site.  
 Area measurements for indicative purposes only.  
 © Carter Jonas. Quality Assured to BS EN ISO 9001:2015  
 Source: Ordnance Survey

**LEGEND**  
 Study area boundary  
 Abingdon Town Centre boundary



Plan D: CARF project study area

## 4. COMMUNITY VIEWS ON PLANNING ISSUES

### Community engagement

4.1 Putting together a Neighbourhood Plan for Culham started with a community-led Plan in 2014. That Plan led to a set of proposals which formed the subject of a community survey. The survey was prepared by Culham Parish Council and members of a specially created Neighbourhood Plan Committee in early 2020. The Neighbourhood Plan Committee continued to meet during 2020 and 2021, providing monthly updates at Parish Council meetings, and adding further details and proposals to the survey results. In August 2021, the Committee had a stall at the Culham Festival, with engagement from villagers and other festival-goers. Finally, there was consultation with the project planners for the redevelopment of the Waggon and Horses, who are key stakeholders in a prominent building, currently boarded up, at the entrance of the village.

### 2014 Community-led Plan

4.2 Key topics identified by the 2014 Community-led Plan were:

- Cycling – improving existing paths; adding strategic links to neighbouring communities
- Traffic – concerns about safety on Tollgate Road and A415; parking around the school
- School – Village School as part of the community; buildings could be more widely used (evenings / out of term)
- Pub – a village asset; sometimes noisy
- Shop – a village shop is desirable
- Church – could be enhanced to allow for a community space
- General – keeping the village a village; developing the mushroom farm (now defunct); investing in hydropower at the weir.

Several of these items are now out of date, such as traffic issues around the Primary School (closed in 2019), use of the Pub (Waggon and Horses, now closed and unoccupied), and a proposal to lobby for the village to be joined to mains gas.

### 2020 Survey

4.3 The Survey was put together explicitly to convert ideas from the Community-led Plan into draft policies for the Neighbourhood Plan and to elicit further feedback from the community. It was posted to all residents and the results suggested not only the material for policies presented in this document, but also for Community action campaigns beyond the scope of planning.

### Survey topics

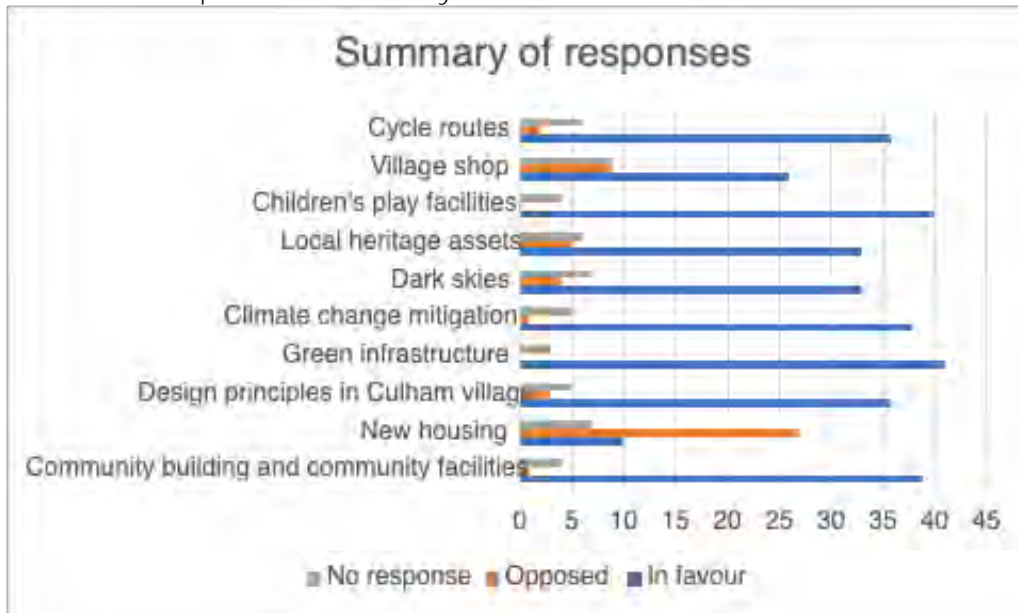
4.4 The topics for the Survey were:

- **Community Building and Community facilities:** "The creation of a community building will be supported, provided the improved / updated / new buildings or structures are designed and sited in such a way that their impact on the open nature, rural character and visual amenity of the Green Belt is minimised and that they will sustain and enhance the village"

- New Housing: "Do you think new housing should be promoted with the village of Culham?"
- Design principles in Culham village: "Do you think setting out design principles which any planning applications would be required to follow is desirable?"
- Green Infrastructure: "Do you think a policy on green infrastructure should be included?"
- Climate Change Mitigation: "Do you think a policy for climate change mitigation regards new housing should be promoted within the NP?"
- Dark Skies: "Do you think a dark skies policy should be included in the Culham NP?"
- Local Heritage Assets: "The NP can identify buildings and structures which are not nationally listed with a view to assess them for local listing (...). This gives them additional protection from being altered or obscured should any planning application be made in the future."
- Play facilities for children of the parish: "The village has a play area on the recreation ground which is owned by the Diocese of Oxford. The NP could ask to ensure that any development the Diocese might have in mind for this land in the future would have within it a playground."
- Village shop: "The NP could encourage the creation of a village shop."
- Cycles routes: "The NP could encourage the creation of cycle routes to Abingdon and to Sutton Courtenay."

## Survey results

4.5 There were 44 replies to the Survey and this is a visual breakdown:



Respondents often included comments and sometimes suggestions, and they shed some light on the motivations for these results.

4.6 Key negative feedback:

- strong aversion to new housing is linked to appreciation of huge impact of STRAT9 development
- ambivalence around village shop (is it viable? / where could it be sited?)
- uncertainty around local heritage: seen as restrictive, additional regulation

- a few questions on whether dark skies policy is necessary or a side issue
- several mentions of failure of previous applications for community facilities

#### 4.7 Key supportive comments:

- near universal approval for green infrastructure and climate change mitigation
- wide approval for more attention to children's play facilities
- requests for allotments as part of green infrastructure and community facilities
- village shop ideas conflated with village pub / reuse of Waggon and Horses
- comments requesting cycle routes to be off-road and provide additional integration with neighbours (Abingdon, Sutton Courtenay, Radley then Oxford)

#### Neighbourhood Plan Committee

4.8 For the most part, the NP Committee discussions continued to reflect the strength of feeling among villagers as to the impact of the STRAT9 development and also repeated the survey results. Another key theme did emerge, most likely driven by the increased pull of the Europa School, with the arrival of a greater number of families with young children: focus on facilities for families. This highlighted the existing demand for adequate communal play facilities, but it also raised the problems of access to the Europa School from the village, and the lack of safe and properly integrated pedestrian flow across the parish. While the absence of a pedestrian crossing on the A415 does not constitute a planning issue, it did come up more than once in Committee meetings and discussions with villagers. A general point, frequently repeated, was that any facilities such as a school, shop, community centre, or local employer, if sited within STRAT9 must be made easily accessible on foot from the village, if only to deter further car use.

#### Culham Summer Festival 2021

4.9 Neighbourhood Plan Committee members held a stall for the day at the Culham Festival on a mostly sunny Saturday 7 August, and this served as an opportunity to raise awareness among villagers about the NP and gather further feedback on the issues that villagers cared most about.

4.10 Again, this was an occasion where strong opinions were voiced about the manner in which villagers felt that STRAT9 had been imposed, with the addition of mounting concern at the lack of consultation on the planning of the new road link from Didcot through our neighbours in Appleford.

4.11 In terms of specific contributions not already discussed, several residents of Tollgate Road expressed concern at the dilapidated state of the former Waggon and Horses and were very keen to hear about the site's future prospects. They were universally supportive of any proposed development that would replace what is now a near derelict building.

4.12 There were also several requests in support of enhancing the cycle route network and improving pedestrian access across Culham. Lockdown lifestyles have left their mark, with villagers seeing increased value in having a wide network of

walkable and cyclable paths across a natural resource that became very dear to them while national travel was restricted.

4.13 The Committee and the Parish Council considers that the policies in the Neighbourhood Plan deals with the matters of greatest concern to the local community in a way that the planning system allows.



## 5. VISION, OBJECTIVES & LAND USE POLICIES

### Vision

5.1 The vision of the neighbourhood area in 2041 is:

*“Culham will have grown successfully as a community through sustainable infill within the existing fabric of the village whilst protecting the Green Belt. These schemes have been well-designed to ensure the rural character of the village has been preserved and the character and significance of the Conservation Area has been sustained and enhanced. Although change in the wider parish has been significant, it has provided the opportunity for access to new community facilities and services and improved connectivity of the wider green infrastructure network from the village into the countryside.”*

### Objectives

5.2 The key objectives of the Neighbourhood Plan are:

- To manage the incremental growth of the village through sensitive infill and to protect the surrounding countryside from harmful development
- To conserve the special heritage character of the village and its landscape setting
- To protect and improve the ecological value and connectivity of the green infrastructure assets of the village and wider parish
- To sustain community facilities and services that are essential to community life

### Land Use Policies

5.3 The following policies relate to the development and use of land in the designated Neighbourhood Area of Culham. They focus on specific planning matters that are of greatest interest to the local community.

5.4 There are many parts of the Parish that are not affected by these policies, and there are many other policy matters that have been left to the Local Plan to cover. This has avoided unnecessary repetition of policies between the two plans, though they have a mutual, helpful inter-dependence.

5.5 Each policy is numbered and titled, and it is shown in bold italics. Where necessary, the area to which it will apply is shown on the Policies Map attached to the document. After each policy is some supporting text that explains the purpose of the policy, how it will be applied and, where helpful, how it relates to other development plan policies.

## Policy CUL1: Community Facilities

A. *The Neighbourhood Plan identifies the following community facilities, as shown on the Policies Map:*

- i. *Culham Recreation Ground & Playground\**
- ii. **St Paul's Church**
- iii. *The Green\**
- iv. *Culham Lock Amenity Space\**
- v. *Culham Lock Car Park*
- vi. *Abingdon Vale Cricket Club and Ground\**
- vii. *Abingdon Town Football Club and Ground\**
- viii. *Europa School*
- ix. *The Railway Inn*
- x. *Waggon and Horses Public House*
- xi. *Andersey Island Water Meadow\**
- xii. *Rye Farm Car Park*

*\* Existing Open Space, Sport and Recreation Facilities*

B. *Development proposals which would affect the use of the identified community facilities of the policy will be determined against the provisions of Policies CF1 (Safeguarding Community Facilities) and CF4 (Existing Open Space, Sport and Recreation Facilities) of the SODCLP.*

C. *Proposals to change the use of part of a community, open space, sport or recreation facility that is surplus to requirements will be supported where they will not undermine the overall viability and importance of the community, open space, sport or recreation facility concerned.*

D. *The comprehensive masterplan for the strategic allocation STRAT9 Land adjacent to Culham Science Centre will be expected to demonstrate that the masterplan layout locates the required social infrastructure in such a way to ensure that the existing village of Culham can access that infrastructure safely and securely.*

5.6 The policy supplements and refines existing development plan policies on community, open space, sport or recreation facilities to which the policies should apply and by seeking to ensure that the long-term potential value of land in community use is not lost without good reason. Given the small nature of the village, the loss of any of these facilities would be significantly detrimental, so even though a specific type of community use may no longer be viable, the opportunity to retain the premises or land in this use cannot be lost. The policy also refines Policy CF1 to ensure its application to the sui generis use of Culham Lock and Rye Farm Car Park.

5.7 In addition to existing development plan policies which protect these facilities and encourages new or extended facilities, the policy also allows for a partial change of use of a facility, if this is intended to help secure its longer-term viability. This may be an important way of putting to use space that is no longer needed, but

which can make a financial contribution to sustaining the facility. However, such changes must be shown not to undermine the community functions of the use.

5.8 The Use Class Order of September 2020 now deems such these uses as either Class F2 ('Local Community Uses') or in the case of St Paul's Church and the Europa School, F1 ('Learning and non-residential Institutions'). The pubs and car parks are now deemed 'sui generis' (i.e. not included in any class of uses). A description of each facility and its community value is provided in Appendix A. Collectively, these facilities are cherished by the community and offer a valuable and vital resource to support community life, and therefore warrant the protection of policies. In some cases, the facility is included in Policy CUL6 as a Local Heritage Asset to identify its historic as well as social value. Or it may be included in Policy CUL7 as a component of nature recovery.

5.9 The policy also takes the opportunity to make sure the social infrastructure of the nearby strategic allocation is located and connected to the village in such a way to ensure it benefits from that infrastructure. The distance from the existing village settlement to the proposed social infrastructure, and sustainable travel routes connecting these two locations, including the improvement of pedestrian and cycling access across the A415 Abingdon Road, will need to be considered in the masterplan layout.

#### Policy CUL2: Former Waggon & Horses Public House

*Proposals which directly provide or assist in the provision of a public house or drinking establishment use at the former Waggon and Horses Public House, as shown on the Policies Map, will be supported, provided:*

- i. A public house or drinking establishment use is retained on the site in line with the provisions of Policy CUL1 Community Facilities;*
- ii. The scheme makes provision for publicly accessible green amenity space;*
- iii. The location and design of any new buildings and structures are such that their height and bulk will not significantly harm the openness of the Green Belt;*
- iv. The building materials and detailing reflect those characteristics of the local area and comply with the requirements of Policy CUL5 Design Code for Culham Village;*
- v. Existing mature boundary trees of value and mature hedgerows are retained, unless removal is required to provide access, and new planting is undertaken, to provide adequate screening of the site within the surrounding landscape;*
- vi. A biodiversity strategy is prepared that creates new opportunities to improve **biodiversity and delivers a measurable 'net gain' in biodiversity value across the site;***
- vii. Pedestrian and cycling access is provided off Tollgate Road to a specification to be agreed with the Highways Authority, and the layout has regard to the need to enhance pedestrian and cycle connectivity across the A415 Abingdon Road including opportunities to enhance connections as defined in Policy CUL8 Sustainable Travel;*

- viii. *Vehicle access is made from Tollgate Road in a location and type to be agreed with the Highways Authority, and suitable for waste and emergency vehicles;*
- ix. *It can be demonstrated that parking provisions would comply with the requirements of Local Plan Policy TRANS5; and*
- x. *It can be demonstrated that lighting proposals would comply with the requirements of Policy CUL10 Light Pollution.*

*Inappropriate development for a Green Belt location will only be supported as part of proposals to redevelop this site if very special circumstances can be demonstrated.*

5.10 The policy is intended to help resolve the future of a prominent brownfield site in the Green Belt on the edge of the village by encouraging proposals to improve and extend the former Waggon and Horses Public House through its redevelopment. In doing so, the policy refines the intent and purpose of SODCLP Policies EMP10 and EMP11, CF1 and CF2. The policy is intended to operate alongside the provisions of strategic (STRAT6) and national policies (§148) on the Green Belt. It may also be possible to design proposals that are not inappropriate development (as per the exceptions of NPPF §149(c)(d) or (g)). In any event, the Parish Council considers the public benefit evidenced by the local community interest in seeing the retention of a viable social asset on the edge of the village, in the absence of any such other nearby, as highly desirable and should accrue significant weight in the planning balance.

5.11 The Waggon & Horses site comprises the former public house building, associated hardstanding previously used for car parking and associated open space. The public house dates back to the late 19<sup>th</sup> century however it was forced to close in 2015 due to no longer being a viable business. The building has since been boarded up and vehicular access from Abingdon Road closed off. Overall, the site does not make a positive contribution to the area and has become an eyesore in a prominent location at the entrance to the village. The current building is not fit for purpose in its current state and requires significant refurbishment in order to bring it up to acceptable standards. The cost of such refurbishment would have a significant impact on the viability of a reopened public house business.

5.12 It has been established through discussions between the landowners and SODC that some form of additional development will be required in order to make the reopening of the public house viable. A hotel element is considered to be a suitable form of additional development for this location. Due to the Green Belt location of the site, however, the quantum of additional development needs to be minimised as far as possible (in viability terms) to avoid sprawl and impact on the openness of the Green Belt. It is therefore more efficient for the public house and hotel to be combined into a single new building to minimise sprawl of development across the site. The existing public house building would also not be able to support the hotel element as an extension to the building. The redevelopment scheme proposes to rebuild the public house element on broadly the same footprint as the existing building with the hotel element perpendicular to it (parallel to Tollgate Road) to give the illusion of the site comprising just the public house element when viewed from Abingdon Road.

5.13 The development principles set out in the policy will secure a sustainable scheme that will benefit the village and wider Parish. In the first instance it will be very important for the public house use to be retained in any future proposal and the policy therefore secures this in line with the provisions of Policy CUL1 Community Facilities. **The landowner has also confirmed that it is intended to retain the 'Waggon and Horses' name for the redevelopment scheme.**

5.14 It should also be possible to deliver some publicly accessible green space as part of proposals to enable the local community to use the space and the inclusion within it of a community orchard will be particularly supported. The landowners have confirmed that an area of land to the south of the site has been reserved for a community orchard, as shown on the Policies Map. It will also be important to retain and enhance landscape and biodiversity provisions on site hence the policy makes provision for these matters.

5.15 Given the proposed strategic development of STRAT9, and the use of the existing Europa School by the existing village community, proposals to improve the pedestrian and cycling access across the A415 Abingdon Road will be crucial. It is also anticipated that the site will become an important meeting place for the existing village and new community. The policy therefore makes provision for the importance of this crossing to be considered in the provision of pedestrian and cycling connectivity as well as other opportunities highlighted in Policy CUL8 Sustainable Travel.

5.16 The site has a long planning history and previous investigations has shown that a new multi-modal access approach to the site, which includes removal of vehicular access from the A415 Abingdon Road and relocating the vehicular access on Tollgate Road away from the traffic signal-controlled junction, will provide betterment over the historic vehicular access from the A415 Abingdon Road and the Parish Council supports this approach. The policy therefore makes provision for this approach with details to be agreed with the Highways Authority.

5.17 SODCLP Policy TRANS5 sets out appropriate parking provisions and other infrastructure requirements for all types of development and the policy therefore directs any future proposals to its requirements. The Parish Council are also mindful that any new scheme may include new lighting proposals and therefore directs any future proposals to the requirements of Policy CUL10 Light Pollution.

### Policy CUL3: Early years provision

- A. *Proposals to retain and improve the following early years provision facilities, as shown on the Policies Map, will be supported, provided they accord with development plan provisions on the Green Belt and other development management matters:*
- i. *Culham Village Nursery & Preschool*
  - ii. *Culham Science Centre Nursery & Preschool*
- B. *Proposals to deliver the strategic allocation at Culham under Policy STRAT9 of the Local Plan should safeguard land for the provision of a new creche, day nursery or day centre use in the event that one of the existing uses of Clause A ceases to operate.*

5.18 The policy is intended to protect the only early years provision uses in the Parish from unnecessary loss. Both facilities provide an important service to the community meaning that fewer car trips are necessary to larger towns beyond the Parish. These uses now form part of Class E of the new Use Class Order, and new permitted development rights allow a change of use to residential use without the need for planning permission. In an area of very high land values for housing, such premises are therefore coming under increasing pressure.

5.19 The Parish Council therefore hopes that South Oxfordshire District Council will make an Article 4 Direction for these facilities in the parish to remove those rights with effect from August 2023, enabling such changes to remain in planning control. At the very least until such a time that, the early years provision for the new strategic STRAT9 allocation becomes clearer. The Parish Council will submit a formal request for this following the referendum of this Neighbourhood Plan.

5.20 Whilst the latest South Oxfordshire Infrastructure Delivery Plan April 2020 update anticipates that the additional educational capacity at strategic allocation STRAT9 will include early years provision, and Oxfordshire County Council, as the Educational Authority, confirms that sufficient early years provision will be sought to mitigate the impact of development, the adopted SODCLP Policy STRAT9 makes no specific provision for this use. This may not be needed if existing uses at Clause A are retained. If new provision is secured within the strategic allocation, then Clause B of policy requires this provision to be safeguarded now and permission will be conditioned to require that land to be used only for the purposes of Class E Part (f) of the Use Class Order. This will be kept under review as part of the monitoring of the Neighbourhood Plan and the strategic allocation.

#### Policy CUL4: Enhancing Culham Conservation Area

*Development proposals within the Culham Conservation Area and its setting should preserve or enhance its significance as a designated heritage asset. Features identified as positive characteristics of the Conservation Area and its immediate setting are defined in the Culham Design Code attached as Appendix B, to which all proposals must have full regard.*

#### Policy CUL5: Design Code for Culham

*Development proposals in Culham will be supported provided they have full regard to the essential design considerations and general design principles set out in the Culham Design Code attached as Appendix B.*

5.21 The policies establish the importance of design of new development in the Conservation Area, its setting and the village to maintain and enhance its character. It directs applicants bringing forward proposals in and around the village to the design codes contained in the Culham Design Code attached at Appendix B.

5.22 The Code refines the South Oxfordshire Design Guide, and in due course the Joint Design Guide which will replace the South Oxfordshire Design Guide, and establishes the principles of essential design considerations within three distinct area typologies of the main village settlement as well as certain features of the area outside of the main village settlement. These considerations set out features of each typology that make it distinctive, and the extent of each is defined in the Design Code document. In turn it complements Policies DES1 and DES2 of the SODCLP by highlighting particular characteristics of the Parish. The policies require proposals demonstrate, where relevant to the nature and location of the proposal, that full regard has been paid to the Code. The policies do not advocate pastiche or historic solution, however it is important that any new development demonstrates a connection with local character and place making.

## Policy CUL6: Local Heritage Assets

The Neighbourhood Plan identifies buildings as Local Heritage Assets as included in the Appendix B and shown on the Policies Map, for the purposes of applying development plan policies on non-designated heritage assets:

- i. The Lion, High Street;
- ii. Nos. 7 – 11 The Green;
- iii. 22-23 High Street;
- iv. School House, High Street;
- v. Kiln Cottage;
- vi. Station House;
- vii. The Railway Inn;
- viii. Tollgate Cottage;
- ix. 60 Abingdon Road;
- x. Maud Hales Terrace, Abingdon Bridge;
- xi. Pill boxes (Types FW3/24 The Burycroft; FW3/24C & FW3/28A at Appleford Bridge; FW3/28A at Sutton Bridge; FW3/24C at Sutton Pools; FW3/28A at Zouch Farm and FW3/28A at Tollgate Road).

5.23 The policy designates certain buildings or structures as Local Heritage Assets in order to give them additional protection as heritage assets, in recognition of the important contribution they make to the special character of the Parish for the application of Policy ENV6 of the SODCLP. Policy ENV6 requires a balanced judgement to take place where proposals directly or indirectly affect the significance of a local heritage asset taking into account the scale of any harm or loss and the significance of the asset, in line with §197 of the NPPF. This means that Policy ENV6 will apply to schemes coming forward which may affect the local heritage assets listed in this policy. Culham parish also comprises a rich archaeological landscape within which is contained numerous cropmark complexes denoting its early occupation and use from the prehistoric period onwards. This policy focuses on the built historic environment. Policy ENV6 of the SODCLP will therefore continue to apply to above or below ground archaeological remains as well as other elements of the historic environment.

## Policy CUL7: Nature Recovery and Climate Change

- A. The Parish contains a variety of green and blue infrastructure that provides an environmental support system for the community and wildlife. The Neighbourhood Plan designates this as a Network, as shown on the Policies Map, for the purpose of promoting nature recovery and for mitigating climate change. The Network comprises the Water Meadows between the village and Sutton Pools, Andersey Island Water Meadows, woodland, trees, hedgerows, Culham Brook SSSI and other land of biodiversity value.*
- B. Development proposals that lie within or adjoining the Network are required to have full regard maintaining and improving the functionality of the Network, including delivering a net gain to biodiversity, in the design of their layouts and landscaping schemes.*
- C. Proposals that will harm the functionality or connectivity of the Network will not be supported. Development proposals that will lead to the extension of the Network, which includes the delivery of allotments for the use of the village, will be supported, provided they are consistent with all other relevant policies of the development plan.*

5.24 The policy defines the presence of green and blue infrastructure assets in the Parish. By doing so it supports SODCLP Policies ENV1 – ENV5 on the Environment. Its purpose is linked with the vision and principles of green infrastructure in the district of defining a network of green infrastructure assets in the neighbourhood plan area as a means of providing environmental support for the community and wildlife. These assets will be maintained and added to throughout the Neighbourhood Plan area and will be the means of nature recovery through connecting and improving habitats and sequestering carbon through woodland planting.

5.25 Alongside the Culham Brook SSSI, there is a variety of other priority habitat areas in the Parish. The village itself hosts Traditional Orchard habitats as well as Deciduous Woodland. A number of these form part of the Core Zone of the Draft Oxfordshire Nature Recovery Network prepared by Wild Oxfordshire. A large part of the Parish has also been identified as a Recovery Zone. The purpose of this policy is to identify existing green and blue infrastructure and opportunities to better connect them which may also play a part in delivering the aims of the Recovery Zone of the Draft Oxfordshire Nature Recovery Network carried out collaboratively by a partnership of local nature conservation organisations, led by Thames Valley Environmental Records Centre (TVERC), Wild Oxfordshire and The Berks, Bucks and Oxon Wildlife Trust (BBOWT) and overseen by Oxfordshire's Biodiversity Advisory Group (BAG) and adopted by the Oxfordshire Environment Board (OxEB). Whilst much of the land surrounding the village settlement, and northern parts of the Parish already have Countryside Stewardship Agreements, opportunities identified by the Neighbourhood Plan include additional tree planting on Land adjacent to the Cricket Club Grounds, additional catchment woodland, floodplain woodland and riparian woodland planting opportunities using the 'Working with natural processes to reduce flood risk' evidence base by the Flood and Coastal Erosion Risk Management Research and Development Programme and Environment Agency in February 2021. These opportunities are all shown on the Policies Map and the Parish

Council will seek to work with landowners to realise such opportunities where possible avoiding the loss of the best and most versatile agricultural land.

5.26 The policy therefore requires that all development proposals that lie within the Network, or that adjoin it, should consider how they may improve it, or at the very least do not undermine its integrity of connecting spaces and habitats. The Policy Map shows the full extent of the Network, which allows applicants to determine if their proposals should take this policy into account. Where proposals include provision for landscaping, new means of access or new layouts, there may be an opportunity to relate the land better to the Network, for example in complementing existing biodiversity value through the design of the landscape scheme. At the very least, the policy requires that proposals that will undermine the existing value of the Network will be refused permission.

5.27 The Network will become more valuable over time, and although the majority of these features are physically attached to enable habitat connectivity, some features of the Network are not. This does not devalue their integral biodiversity or recreational value and at some point in the future an opportunity may arise to achieve similar connectivity. The Neighbourhood Plan also signals to the Responsible Authority that it should consider the role of this Network in the future Local Nature Recovery Strategy for the area which is now a requirement of the Environment Bill.

#### Policy CUL8: Sustainable Travel

- A. *The Neighbourhood Plan identifies the existing Sustainable Travel Network, as shown on the Policies Map, for the purpose of supporting active travel in the Parish.*
- B. *Development proposals on land that lies within or adjacent to the Network should sustain, and where practicable, enhance the functionality of the Network by virtue of their layout, means of access and landscape treatment.*
- C. *Proposals that will harm the functioning or connectivity of the Network will not be supported.*
- D. *The comprehensive masterplan for the strategic allocation STRAT9 Land adjacent to Culham Science Centre will be expected to demonstrate that the masterplan layout enables safe and secure access to the required social infrastructure for the existing village of Culham through new, and improvement to, existing cycleways, footpaths, and bus services.*

5.28 The policy seeks to encourage safe, accessible and convenient means of walking and cycling in the parish. It refines Policy TRANS2 by providing a local element to its provisions. The policy implements elements of the Culham Community Led Plan in 2014 which identified cycle safety as being important especially between Culham and Abingdon. At the time the Culham Centre for Fusion Energy was interested in improving arrangements for their cycling employees. Ideas that came from the consultation were:

- 1) A cycle path from Waggon and Horses to Abingdon

- 2) Cyclists able to use the Causeway with pedestrians
- 3) Road to Abingdon is too dangerous – use the causeway
- 4) Purchase land to the south of A415 for a new pavement/cycle path
- 5) Improve cycling to Sutton Courtenay
- 6) Footbridge from Culham to Abingdon marina
- 7) Cycle path alongside the railway to Oxford via Radley joining Sustrans Cycle Route 5 including a river crossing

5.29 The Policies Map shows the full extent of the Network, which allows applicants to determine if their proposals should take this policy into account. Where proposals include provision for landscaping, new means of access or new layouts, there may be an opportunity to relate the land better to the Network and/or improve the attractiveness of rural routes. At the very least, the policy requires that proposals that will undermine the existing value of the Network will be refused permission.

5.30 The extension of existing paths has been identified as opportunities to improve the Network and these are shown on the Policies Map at the end of this document. There are also a number of permissive paths which landowners have allowed the local community to use to enjoy the surrounding countryside and improve local connectivity and the Parish Council will continue to work with local landowners in this respect. The policy also signals that connectivity of the existing village to the strategic allocation STRAT9 Land adjacent to Culham Science Centre will be important to ensure that the existing community can benefit from the provisions in the new development. The crossing at the traffic lights on the A419 Abingdon Road is already problematic making very little provision for safe and convenient pedestrian and cycling access. 8) Improvements to this crossing and the existing shared pedestrian and cycleway on the A419 Abingdon Road will be expected to form part of the provision of sustainable transport facilities required by Policy STRAT9.

## Policy CUL9: Zero carbon buildings

- A. **All development must be 'zero carbon ready'** by design to minimise the amount of energy needed to heat and cool buildings through landform, layout, building orientation, massing and landscaping. Consideration should be given to resource efficiency at the outset and whether existing buildings can be re-used as part of the scheme to capture their embodied carbon.
- B. Wherever feasible, all buildings should be certified to a Passivhaus or equivalent standard with a space heating demand of less than 15KWh/m<sup>2</sup>/year. Where schemes that maximise their potential to meet this standard by proposing the use of terraced and/or apartment building forms of plot size, plot coverage and layout that are different to those of the character area within which the proposal is located, this will be supported, provided it can be demonstrated that the scheme will not have a significant harmful effect on the character area.
- C. All planning permissions granted for new and refurbished buildings should demonstrate that they have been tested to ensure the buildings will perform as predicted and will include a planning condition to require the provision of a Post Occupancy Evaluation Report to the Local Planning Authority within a specified period, unless exempted by Clause B. Where the Report identifies poor energy performance and makes recommendations for reasonable corrective action, the applicant must demonstrate that those actions have been implemented before the condition will be discharged.
- D. All planning applications for major development are also required to be accompanied by a Whole Life-Cycle Carbon Emission Assessment, using a recognised methodology, to demonstrate actions taken to reduce embodied carbon resulting from the construction and use of the building over its entire life.
- E. An Energy Statement will be submitted to demonstrate compliance with the policy (except for householder applications). The statement will include a passive design capacity assessment to demonstrate how opportunities to reduce the energy use intensity (EUI) of buildings over the plan period have been maximised in accordance with the energy hierarchy. Designers shall evaluate the operational energy use using realistic information on the intended use, occupancy and operation of the building to minimise any performance gap.

5.31 The policy context for encouraging higher energy efficiency standards at the Local Plan or Neighbourhood Plan scale is complex. Background information has therefore been set out in Appendix A. The policy may also appear rather technical, but it is a temporary measure as in due course, it is expected that the new Local Plan, if not national policy itself, will make such provisions across the District.

5.32 This policy is in five parts, the combination of which is intended to deliver a step change in the energy performance of all new developments in the Parish and, in doing so, encourage and incentivise the use of the Passivhaus or equivalent standard of building design. Along with the passive design capacity assessment, it is

anticipated that designers will demonstrate compliance using a design for performance methodology such as the Passivhaus Planning package or CIBSE TM34 Operational Energy. Achieving this level of performance will make a significant contribution to mitigating climate change that the Neighbourhood Plan can deliver.

5.33 Clause A of the policy requires developers to ensure they address the **Government's climate change targets and energy performance at the very initial stages of design. 'Zero Carbon Ready' by design** means making spatial decisions on layout and orientation of buildings at the outset to maximise the passive design benefits ('free heat') of a site and avoids leaving this to technical choices and assessment at the Building Regulation stage, by which time the opportunity may have been lost. In the absence of supplementary guidance from the District Council, applicants are directed to the Net-Zero Carbon Toolkit created by Cotswold District Council and two partner councils, West Oxfordshire District Council and Forest of Dean District Council. The toolkit is available as a resource for private and public sector organisations to use and adopt. ([Link](#))

5.34 Its Clause B requires all schemes, no matter what their intended use or size other than householder extensions, to use the Passivhaus Planning Package (PHPP) or equivalent design methodology for all buildings where it is feasible to do so. This means that the applicant must demonstrate those factors that make its use unfeasible, for example, the topography and orientation of the site.

5.35 In respect of scheme viability, any extra-over cost of building to the **'zero carbon ready' Passivhaus standard (now less than 5%)** will diminish to zero well within **the period of this Plan, as per both the Government's Regulatory Impact Assessments**, research by the Passivhaus Trust and the viability assessment published by Cornwall Council. The policy will also ensure that expensive and unnecessary retrofit costs are not passed down to building occupiers in the future, particularly in an area which has relatively high property values. Scheme viability will not therefore be acceptable as a reason for not using the Standard, unless the applicant can demonstrate the scheme has abnormal costs to accommodate.

5.36 The policy requires that the scheme density (measured by dwelling units/Ha) is **assessed against that of the local 'character area' in the Design & Access Statement**. The Culham Design Code defines the key design principles for the Parish. **Outside of such areas, the applicant may define the 'character area' that is relevant for the purpose of this exercise.**

5.37 Proposals seeking to apply the PHPP must be able to demonstrate that the **Passivhaus standard can be achieved**. Prior to commencement a **'pre-construction compliance check' completed by a Passivhaus Designer accredited by the Passive House Institute (PHI)** will be required and secured by condition. Upon completion a Quality Approved Passivhaus certificate for each building will be required prior to occupation, again secured by condition.

5.38 Clause C requires the developer of a consented housing development scheme of any size to carry out a Post-Occupancy Evaluation (POE) including actual metered energy use, and to submit the report to the local planning authority. It will be implemented by attaching a planning condition, which will only be discharged once the report has been submitted and any recommended actions to rectify any

performance gap with the design stage assessment are carried out by the developer. Passivhaus certified schemes will not fail in this way and they are therefore exempted from this policy requirement. In the absence of supplementary guidance from the District Council on POE, guidance has been included in Appendix D.

5.39 Clause D requires all development proposals that are not householder applications to be accompanied by a Whole Life-Cycle Carbon Emissions Statement. RICS ([Link](#)) methodology must be followed as per the Joint Design Guide of SODC. The assessment will enable the design team to understand and respond to the lifetime consequences of their design decisions and to design for adaptability, longevity and disassembly; contributing to resource efficiency (Clause A) and contributing to the 'circular economy' ([Link](#)). This requirement will be added to SODC's Validation Checklist for outline and full planning applications applying to proposals in the neighbourhood area until such a time that there is a district-wide requirement.

5.40 Clause E requires an Energy Statement to be submitted to cover the following:

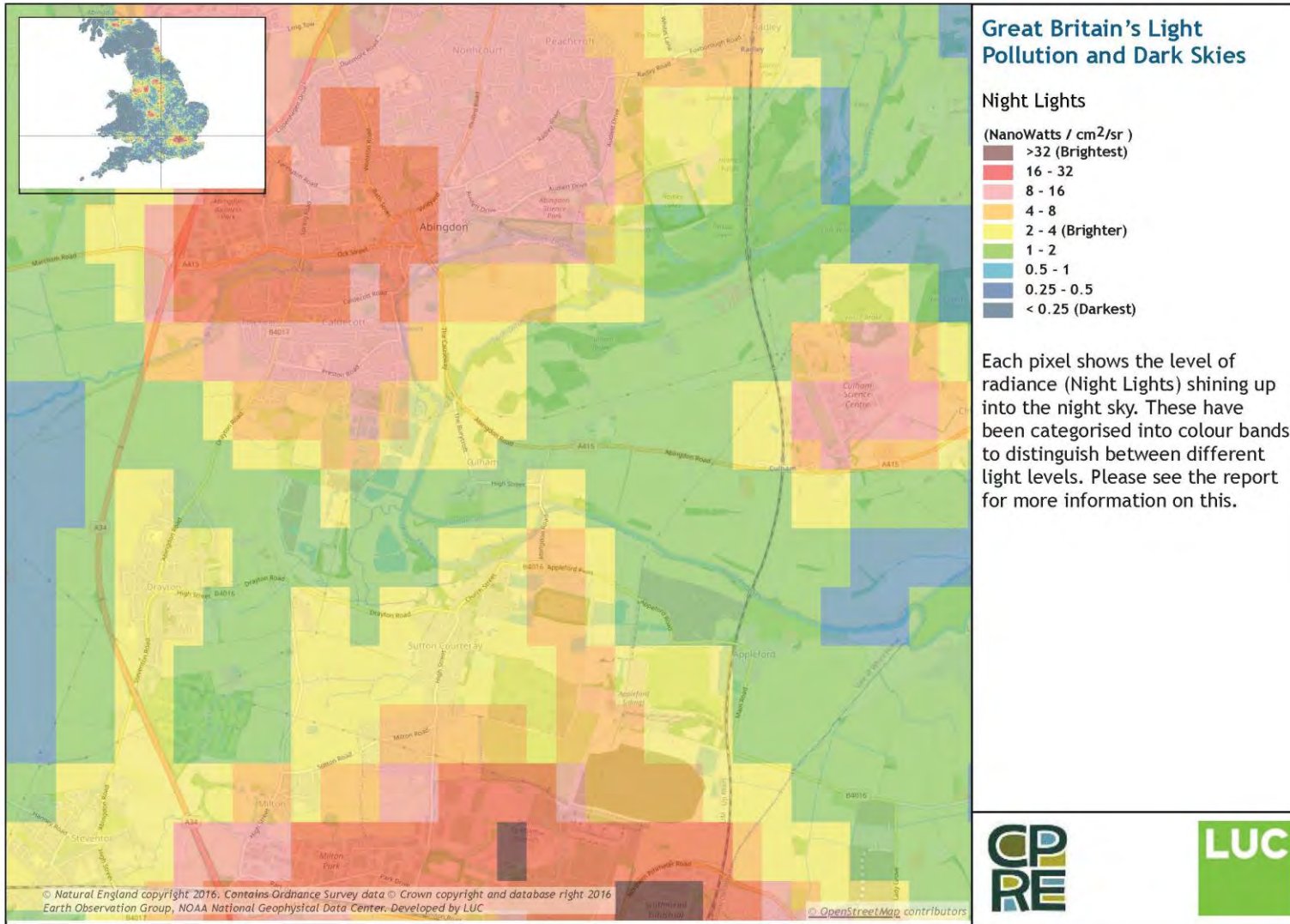
- o an assessment of the proposal to minimise regulated and unregulated emissions, the embodied emissions and the emissions associated with maintenance, repair and replacement of the new building(s), as well as its dismantling, demolition and eventual material disposal
- o a calculation of the energy and carbon emissions covered by the Future Homes Standard and Building Regulations and, separately, the energy demand and carbon emissions from any other part of the development that are not covered by the Future Homes Standard or Building Regulations
- o the proposal to reduce carbon emissions beyond the Future Homes Standard and Building Regulations through the energy efficient design of the site, buildings and services
- o the proposal to further reduce carbon emissions through the use of zero or low emission decentralised energy where feasible
- o the proposal to further reduce carbon emissions by maximising opportunities to produce and use renewable energy on-site, utilising storage technologies where appropriate
- o the proposal for a demand-side response, specifically through installation of smart meters, minimising peak energy demand and promoting short-term energy storage
- o an analysis of the expected cost to occupants associated with the proposed energy strategy

5.41 Every new build or redevelopment project in the Culham Neighbourhood Plan Area provides an opportunity to make a difference and a contribution towards meeting our climate change targets for 2050. This new information requirement need not be an unreasonable expectation of even the smallest schemes for new buildings. Land values in the Culham area are high relative to build costs and ought to be sufficient to ensure requirements to tackle improving energy and carbon performance are viable.

### Policy CUL10: Light pollution

- A. *All development proposals should be designed to minimise the occurrence of light pollution. Development proposals should employ energy-efficient forms of lighting that also reduce light scatter and have regard with the current guidelines established for rural areas by the Institute of Lighting Professionals (ILP).*
- B. *Proposals for all development will be expected to demonstrate how it is intended to prevent light pollution. Information on these measures must be submitted with applications, and where a development would potentially impact on light levels in the area, an appropriate lighting scheme will be secured by planning condition.*

5.42 The Culham Community Led Plan 2014 identified that less street lighting was important to the local community. The strategic allocations and transport schemes in and adjacent to the parish will have a major impact on existing lighting levels. The brightest levels can already be seen to be straying into the village's night sky (see Plan E below). Many councils across England support measures to protect and enhance the dark night sky. The policy reflects the purpose and objectives of policies ENV11 and ENV12 on Pollution of the SODCLP. For all proposed developments, factors that will be considered when deciding the appropriateness of artificial lighting, include the location, the hours of operation, the quantity of lights proposed, brightness and control, and direction of the beam.



For best results the map should be printed at A4 landscape.

## 6. IMPLEMENTATION

6.1 The Neighbourhood Plan policies will be implemented through the determination of planning applications for development in the Parish by the local planning authority.

### Development Management

6.2 The planning authority will use both the Local Plan and Neighbourhood Plan policies to inform and determine its planning application decisions. The Parish Council is a statutory consultee on planning applications made in the Parish and it will be made aware of any future planning applications or alterations to those applications by the planning authority. It will seek to ensure that the Neighbourhood Plan policies have been identified and applied correctly by applicants and by officers in their decision reports.

6.3 Where necessary, the Parish Council may seek to persuade the Secretary of State to call-in a planning application that it considers is in conflict with the Neighbourhood Plan but which the planning authority has deemed to consent. Similarly, it may also seek to persuade the Secretary of State to recover an appeal of a refused application, where the conflict with one or more Neighbourhood Plan policies has been important in the reasons for refusal. In both cases, the Parish Council will do so if it considers matters of national policy significance (for neighbourhood planning) are raised.

### Local Infrastructure Improvements

6.4 Where opportunities arise through Section 106 agreements (or through the Community Infrastructure Levy) to secure financial contributions to invest in improving local infrastructure, the Parish Council propose that the priorities for investment of future Community Infrastructure Levy, and/or S106 contributions received by the local planning authority are:

- Improving existing community facilities – toilets in the church, improved play equipment
- Traffic Management Measures. The main issues identified from the Culham Community Led Plan 2014 were:
  - i. A pedestrian crossing on the A415 at the end of Burycroft
  - ii. Traffic calming on Tollgate Road
  - iii. A pedestrian crossing on the A415 at the Waggon and Horses
  - iv. Traffic calming on High Street
  - v. Less street lighting
- New allotments to serve Culham village
- A new Culham Village Hall

### Central Abingdon Regeneration Framework

6.5 The Parish Council will engage with the Central Abingdon Regeneration Framework (CARF) project team as part of its day-to-day business. It is not considered that this Neighbourhood Plan contains policies that will harm the delivery of the CARF project. The Parish Council supports the aims of the CARF project. Given

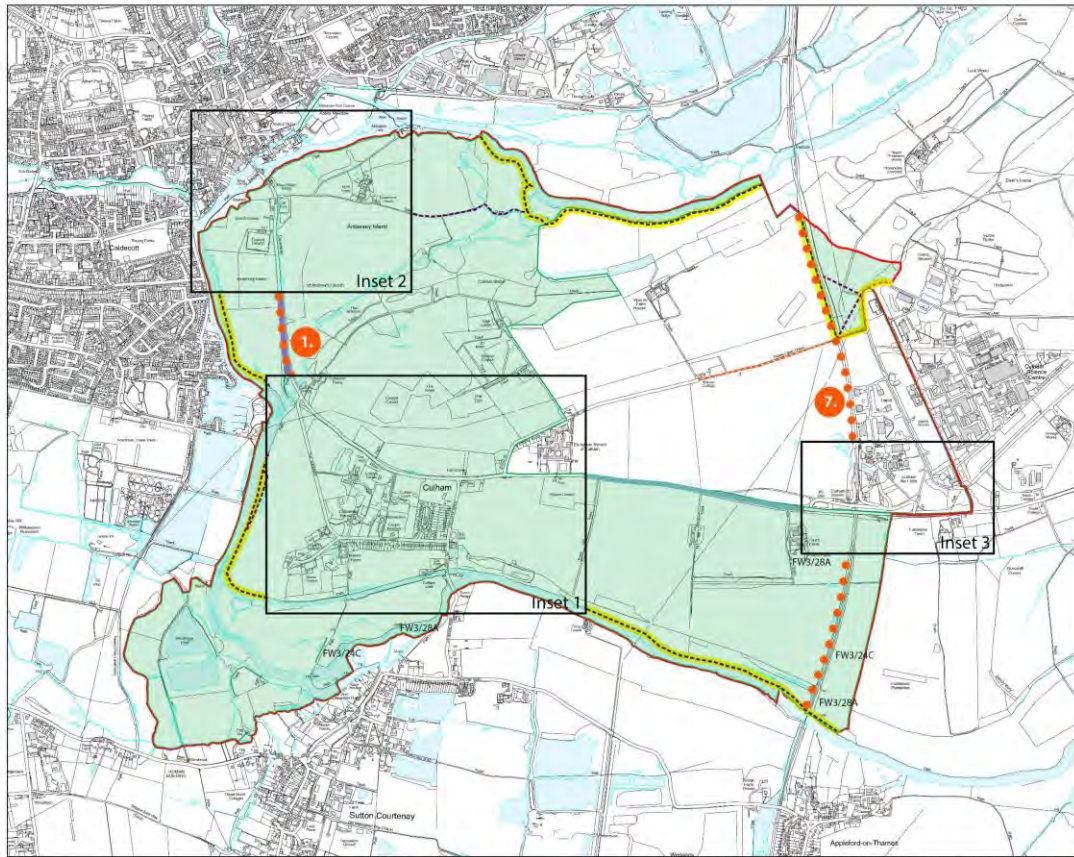
the early stage of the CARF project, and the advanced stage of this Neighbourhood Plan, the Parish Council has committed to considering a modification to this Neighbourhood Plan if it becomes clear that this may be necessary to support the CARF project in due course.

#### Other Non-Planning Matters

6.6 During the process of preparing the Neighbourhood Plan, there have been many ideas for improving or addressing current problems in the Parish that lie outside the scope of the land use planning system to control. The Parish Council has noted these issues and will take them forward through its day-to-day business and in partnership with the local community and relevant parties. These include:

- Research discounts for bulk buying fuel with intention of setting up a scheme or adding to the oil bulk buying scheme that already exists
- Keeping the village tidy
- **Community 'Good Neighbour' Scheme**
- More social events / activities
- Emergency Plan

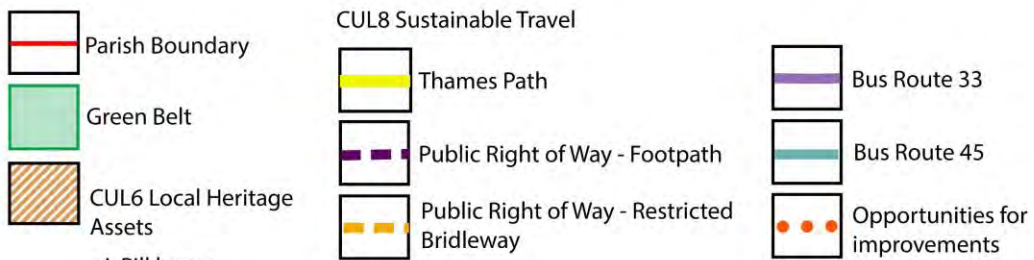
# POLICIES MAP & INSETS



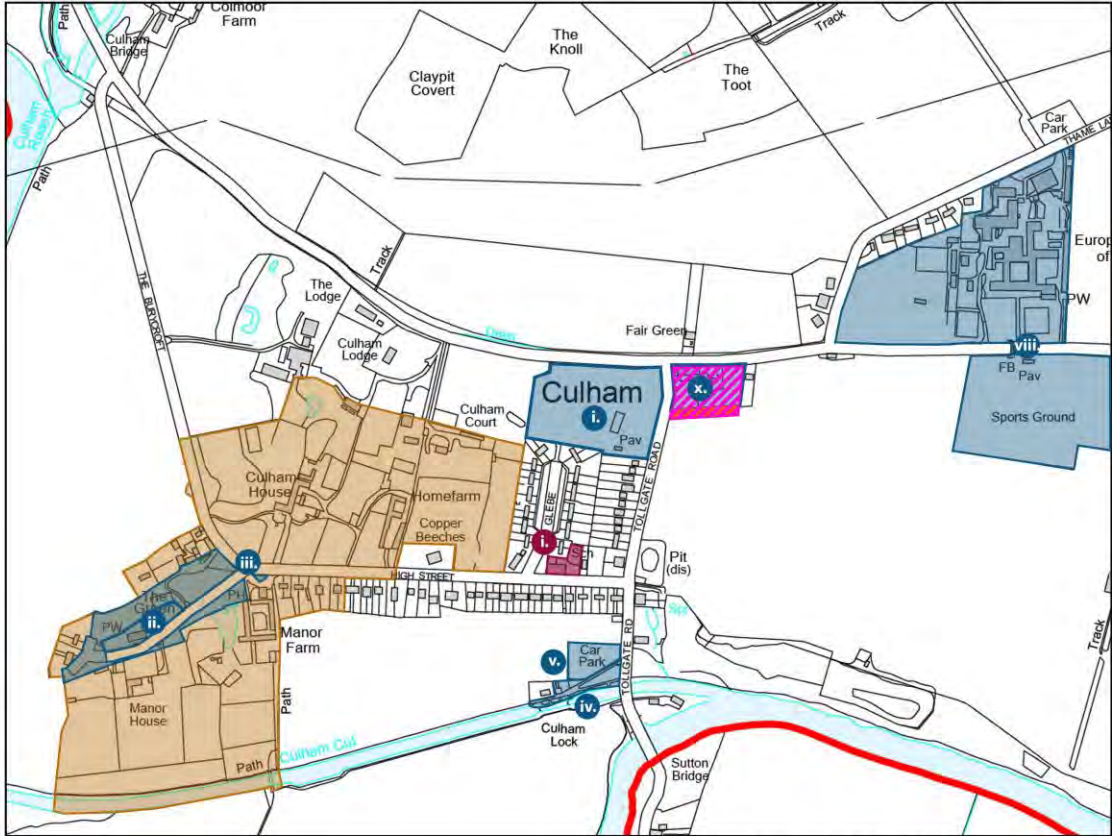
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## Culham Neighbourhood Plan

### Policies Map - November 2021



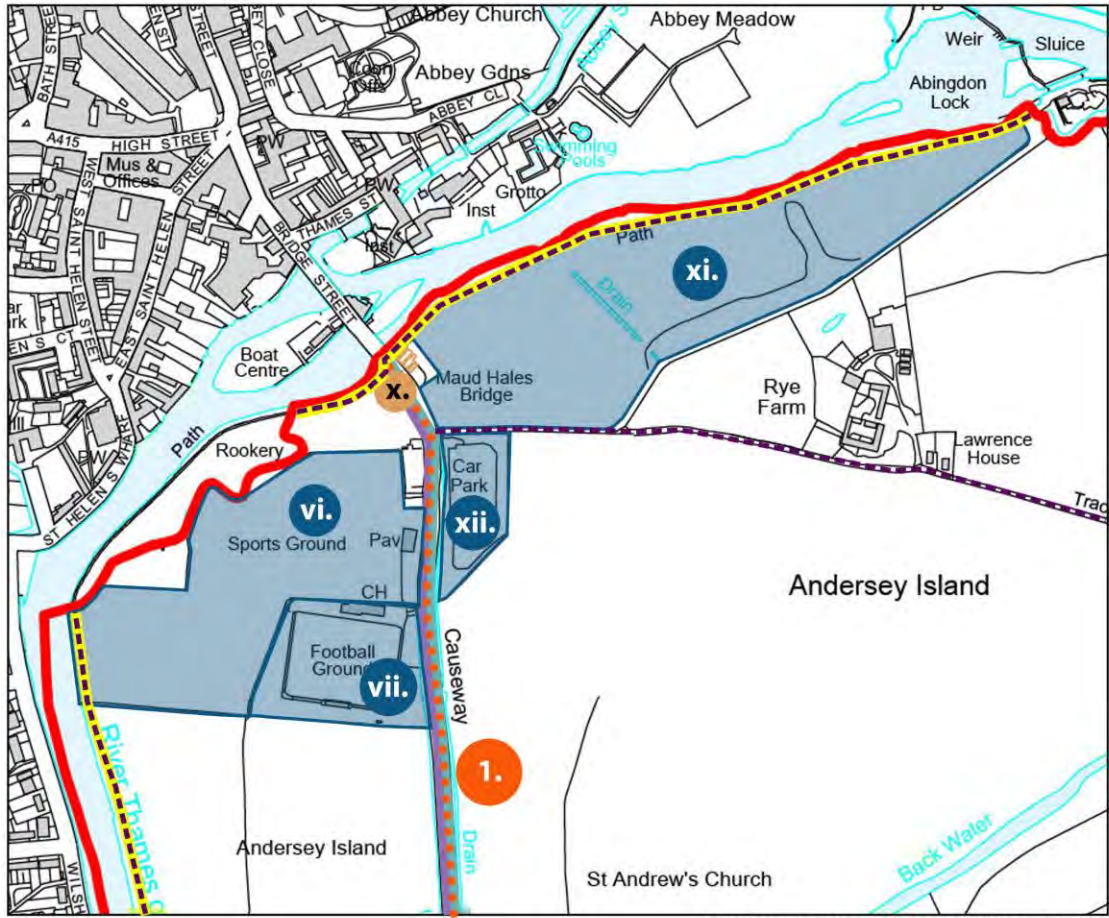
- xi. Pill boxes  
 FW3/24C Appleford Bridge  
 FW3/28A Appleford Bridge  
 FW3/28A Sutton Bridge  
 FW3/24C Sutton Pools  
 FW3/28A Zouch Farm



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### Culham Neighbourhood Plan Policies Map Inset 1 - November 2021

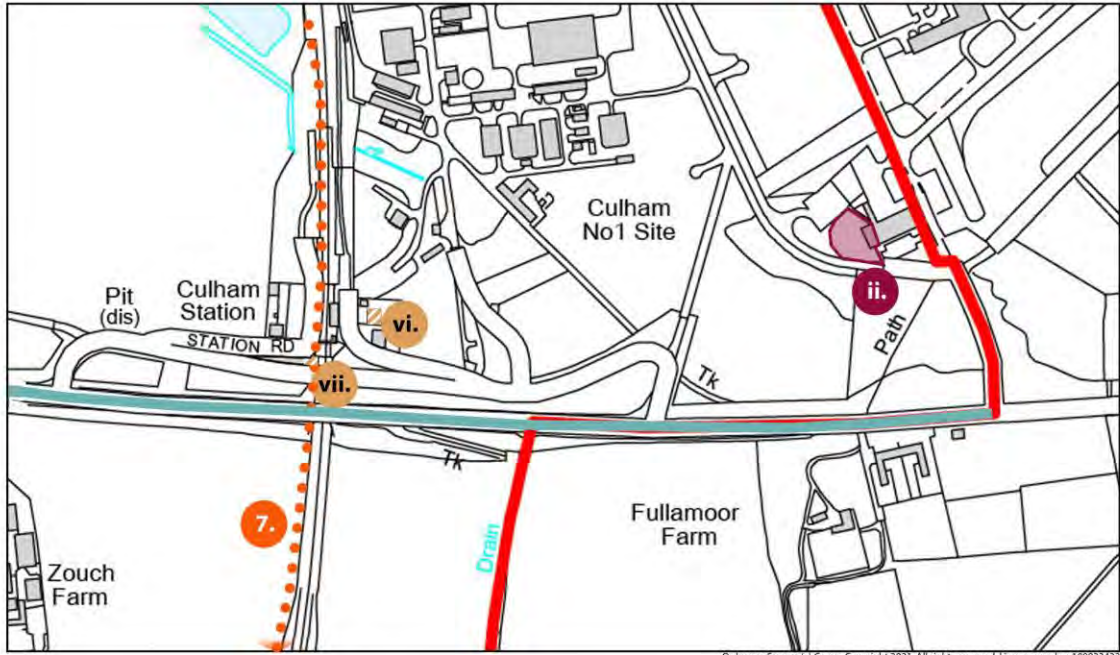
- |  |   |  |
|--|---|--|
|  Parish Boundary  |  CUL3 Early years provision<br>i. Culham Village Nursery & Preschool   |  CUL8 Sustainable Travel<br>Thames Path |
|  CUL1 Community Facilities<br>i. Culham Rec Ground & Playground<br>ii. St Paul's Church<br>iii. The Green<br>iv. Culham Lock Amenity Space<br>v. Culham Lock Car Park<br>viii. Europa School<br>x. Waggon and Horses Public House |  CUL4 Conservation Area  |  PRoW - Footpath                        |
|  CUL2 Former Waggon & Horses  |  CUL6 Local Heritage Assets<br>i. The Lion, High Street<br>ii. Nos. 7-11 The Green<br>iii. 22-23 High Street<br>iv. School House, High Str<br>v. Kiln Cotage<br>viii. Tollgate Cottage<br>ix. 60 Abingdon Road<br>xi. Pill boxes |  PRoW - Bridleway                       |
|  CUL2 Reserved land for community orchard   |  FW3/24 The Burycroft & FW3/28A Tollgate Road  |  Bus Route 33                           |
|  |   |  Bus Route 45                           |
|  |   |  Opportunities for improvements         |



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### Culham Neighbourhood Plan Policies Map Inset 2 - November 2021

- |  |  |
|--|--|
| <ul style="list-style-type: none"> <li> Parish Boundary</li> <li> CUL1 Community Facilities <ul style="list-style-type: none"> <li>vi. Abingdon Vale Cricket Club and Ground</li> <li>vii. Abingdon Town Football Club and Ground</li> <li>xi. Andersey Island Water Meadow</li> <li>xii. Rye Farm Car Park</li> </ul> </li> <li> CUL6 Local Heritage Assets <ul style="list-style-type: none"> <li>x. Maud Hales Terrace, Abingdon Bridge</li> </ul> </li> </ul> | <p>CUL8 Sustainable Travel</p> <ul style="list-style-type: none"> <li> Thames Path</li> <li> Public Right of Way - Footpath</li> <li> Bus Route 33</li> <li> Bus Route 45</li> <li> Opportunities for improvements</li> </ul> |
|--|--|

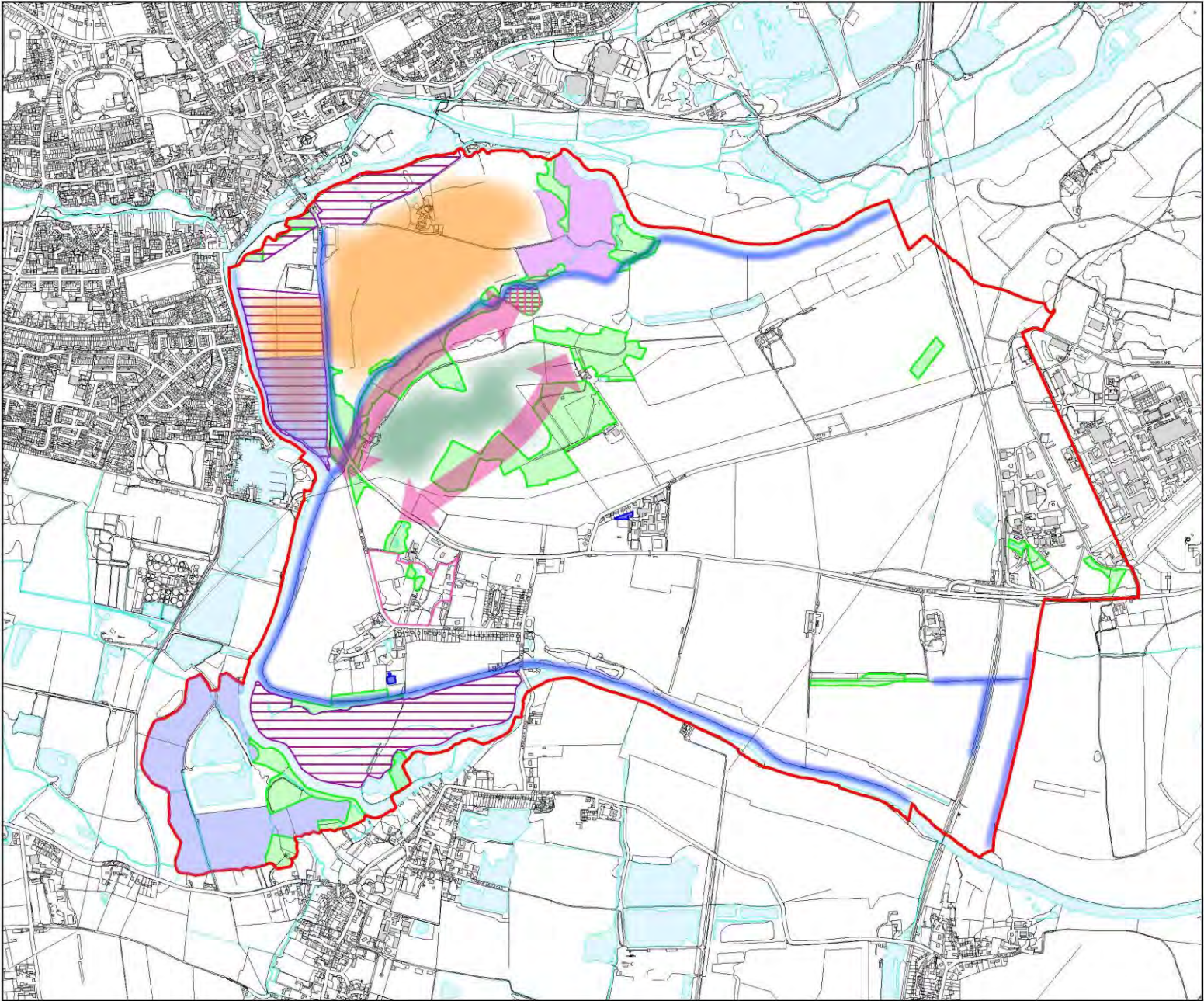


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**Culham Neighbourhood Plan  
Policies Map Inset 3 - November 2021**

-  Parish Boundary
-  CUL3 Early years provision  
ii. Culham Science Centre Nursery & Preschool
-  CUL6 Local Heritage Assets  
vi. Station House  
vii. The Railway Inn
-  CUL8 Sustainable Travel  
Bus Route 45
-  Opportunities for improvements

**Culham Neighbourhood Plan  
Policy CUL7  
April 2022**



- Priority Habitats**
  - Woodland (Deciduous)
  - Traditional orchards
  - Semi-improved grassland
  - Coastal and Floodplain grazing marsh
  - Ancient and semi-natural woodland
- Other Habitats**
  - Garden and Parkland
- Designations**
  - Culham Break SSSI
- Water Meadows**
  - Meadows
- Opportunities**
  - Riparian woodland planting opportunity along water corridors
  - Catchment woodland planting opportunity
  - Floodplain woodland planting opportunity
  - Wildlife Corridors

## APPENDIX A – COMMUNITY FACILITIES

Policy CUL1 and CUL3 seeks to ensure that valued community facilities in the Parish have the proper protection of the planning system. These buildings, and the operational land in which they sit, all have a special value to the local community.

The following facilities are identified by Policy CUL1:

### i. Culham Recreation Ground and Playground



A local community group have been working since September 2018 to develop a new playground at the Recreation Ground in Culham. The existing playground is very limited and the equipment that remains is gradually becoming unsafe and is being removed for safety reasons. The land that the Recreation Ground is on is owned by the Diocese of Oxford and leased to the Parish Council. Currently there are only 9 years left on the lease.

### ii. St Paul's Church

The active core membership of the church has recently increased after a short



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period of decline and the electoral roll in 2005 had 20 names. There is active lay participation in all services, especially with readings and often with intercessions, at the All Age service on the first Sunday of the month, the Evening Prayer service on the third Sunday, at Communion services at Easter and Christmas and at other occasional services. Details of all services by the Bridge Group are printed in the village newsletter, 'The Mouthpiece'

### iii. The Green



Image Source: Oxfordshire Villages

Arthur Beckett's Memories: "There were shops overlooking the village green and after the bread was baked the baker would put villagers' meals into his oven to cook for a 1/2 penny. My mother used to send Doris to the bakers with our food. There were also some thatched cottages on the Green, and I remember that one caught fire and the fire spread to all of them. It was quite a blaze and us children all went to watch as the firemen tried to put out the fire."

iv. *Culham Lock Amenity Space*

The lock is a tranquil place with several benches beneath majestic trees offering plenty of shade. Views of the picturesque Sutton Bridge extension can also be enjoyed.

v. *Culham Lock Car Park*

Culham Lock Car Park, owned and operated by South Oxfordshire Council, is an important resource for the village. Culham Lock is an attractive destination for river users and walkers, its other regular clientele are parents dropping off and picking up children at the schoolhouse every morning and afternoon. The Lock Car Park is therefore an essential community facility, if it were not for the car park then car drivers would be forced to park along Tollgate Road which has traffic lights at both ends, and along Culham High Street. Tollgate Road would quickly become impassable as buses and lorries attempted to pass with traffic queues at both ends of the road, whilst the High Street would become a car park along one side restricting it to a single lane and inhibiting the ability of our local bus to pass through the village. The community as well as valuing the amenity of the carpark is also involved in its upkeep. The boundaries have been improved over recent years, most recently with the addition of a gate from the car park directly to the lock grounds, enabling users to avoid walking along Tollgate Road to get from the car park to the lock. The Parish Council also receives reports at its meeting of any damage to the car park, and will report damage to South Oxfordshire District Council to assist in ensuring it is properly maintained.

vi. *Abingdon Vale Cricket Club and Ground*

Owned by the Vale of White Horse District Council. Adults play league cricket on Saturdays in four teams, covering a wide range of abilities, ages, and experiences. The junior section starts at five years of age. At the time of writing the club has 50 senior members and 125 junior members and looking to expand in 2022 by introducing a women's team across the senior and junior sections.

vii. *Abingdon Town Football Club and Ground*

Affiliated to the Berks & Bucks FA. Teams are currently members of the Hellenic League Division One East and Southern Region Womens Division One.

viii. *Europa School*



Europa School UK is a unique state school, affiliated with the European School system and offering children a bilingual education in French, German or Spanish. Pupils follow both the national curriculum and the European curriculum from age 4 through to 18, culminating in the highly respected European Baccalaureate.

Image Source: Europa School website

ix. *The Railway Inn*



A traditional English pub, restaurant with rooms situated on the A415 Abingdon to Dorchester-on-Thames, adjacent to Culham Railway Station. The pub served takeaway food during the pandemic and is used by local people as well as for short stays from visitors.

Image Source: The Railway Inn website

x. *Waggon and Horses Public House*

Currently in a state of disrepair. This asset could be a valuable resource for the



community if brought back into community use. A planning application for the redevelopment of the Waggon & Horses Public House including extension to dining room and construction of 12 bedroom motel and 8 dwelling houses proposed to financially enable the development is currently being considered by South Oxfordshire District Council (P18/S3067/FUL).

Image Source: Movement80

xi. *Andersey Island Water Meadow*

The Right bank from Old Culham Bridge to half a mile above Abingdon Lock is Andersey Island - bounded by the Swift Ditch backwater weir stream and the main channel of the river. The area is rich in history and well used by local people, for events such as the Abingdon Park Run, dragon boat races etc., takes place here and the area holds special community value.

xii. *Rye Farm Car Park*

Owned by The Vale of White Horse District Council, The Rye Farm Car Park provides free parking for river users for up to two hours.

The following facilities are identified by Policy CUL3:

i. *Culham Village Nursery & Preschool*



Image Source: Day Nurseries

For children from three months to five years old following the Early Years Curriculum the facilities are purposely designed for each child's age and children benefit from a large rural garden, mature trees and a forest school area.

*ii. Culham Science Centre Nursery and Preschool*

For children from three months to five years old following the Early Years Curriculum the facilities are purposely designed for each child's age.

## APPENDIX B – CULHAM DESIGN CODE

# Culham Design Code

June 2022



CULHAM 3

# Contents

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| <b>3. Understanding, Responding to and Applying the Code</b> | <b>4</b>  |
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| Settlements and designations                                 | 14        |
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| <b>5. Design Codes</b>                                       | <b>29</b> |

Front Cover image source: SCLHS | Location – Culham Village - The Green & Burycroft | Photo originally by Percy Simms of Chipping Norton (Victorian photographer)

# 1. What is a Design Code?

Design Codes are tools used to inform the design process of new development. They are prepared through establishing the principles of essential design considerations.

## 2. The purpose of this document

The Design Code Document refines the Joint Design Guide that covers the whole of South Oxfordshire and Vale of Whitehorse Districts. The Joint Design Guide is expected to replace the South Oxfordshire Design Guide 2016 following its adoption. This Design Code Document appraises the main village settlement including identifying important features of the Culham Conservation Area, designated on 11 December 1984, due to all South Oxfordshire District Council records concerning Culham Conservation Area having been lost in the 2015 fire.

The Code has been prepared in accordance with the National Model Design Code and its Guidance Notes published by the Ministry of Housing, Communities & Local Government in July 2021 as relevant to this area and policy context. Its content will inform the Culham Neighbourhood Plan to bring clarity to the definition of the village and the Conservation Area to raise the standards of design for the purpose of managing future infill development proposals and/or rural exception sites.



Culham Design Code  
June 2022



# 3. Understanding, Responding to and Applying the Code

The Joint Design Guide comprises a series of steps. An introduction sets out information about the districts. The Analysis that follows in this Code mirrors this approach tailored to the Parish. The Joint Design Guide then sets out a series of design principles which applicants should adopt as their design goals, where applicable. This Code relates itself to the overarching design principles in a way that reflects the distinct characteristics of the main village settlement.

Applicants preparing development proposals should be familiar with the Joint Design Guide and then relate the proposed development location to the Neighbourhood Area. The District Council will apply the generic and process principles of the Joint Design Guide and the specific requirements of this Code as relevant to the location and nature of the proposal. The Parish Council will use both the Joint Design Guide and the Code to inform their judgement of proposals in making their representations to the District Council when it is consulted on planning applications.

As with all design guidance, the standards and requirements should be regarded as setting the design brief for a proposal, but the applicant may depart from them where it can be justified in the circumstances. Given the Green Belt status of the Neighbourhood Area, for which full regards needs to be paid to national policy, the scope for change in character will remain very limited. However, in all cases, the burden will be on the applicant to demonstrate that the Joint Design Guide and this Code have been acknowledged, understood and responded to in a way that is appropriate to the location and nature of the proposal.

## The Design Guide

- About the guide
- Design and Planning

## Why is design important?

- Its purpose
- We want to...
- Sustainable, high quality places
- The value of good design

## How to use the guide

- Follow the steps
- The design process
- Criteria and principles
- Interactive guide

## Key design objectives

- For all developments

## About South and Vale

- An introduction
- Settlements and designations
- Landscape character

## Place and setting

- Analysis
- Concept

## Natural environment

- Landscape
- Forestry
- Biodiversity

## Movement and connectivity

- Creating a network
- Streets as spaces
- Street design
- Public realm

## Space and layout

- Framework and structure
- Plots and amenity
- Storage, services and utilities
- Parking
- Open space design
- Play space design

## Built form

- General principles
- Apartments
- Householder extensions and outbuildings
- Rural low density
- Building conservation and conversions
- Non-domestic buildings
- Mixed use development
- Materials, maintenance and management

## Climate and sustainability

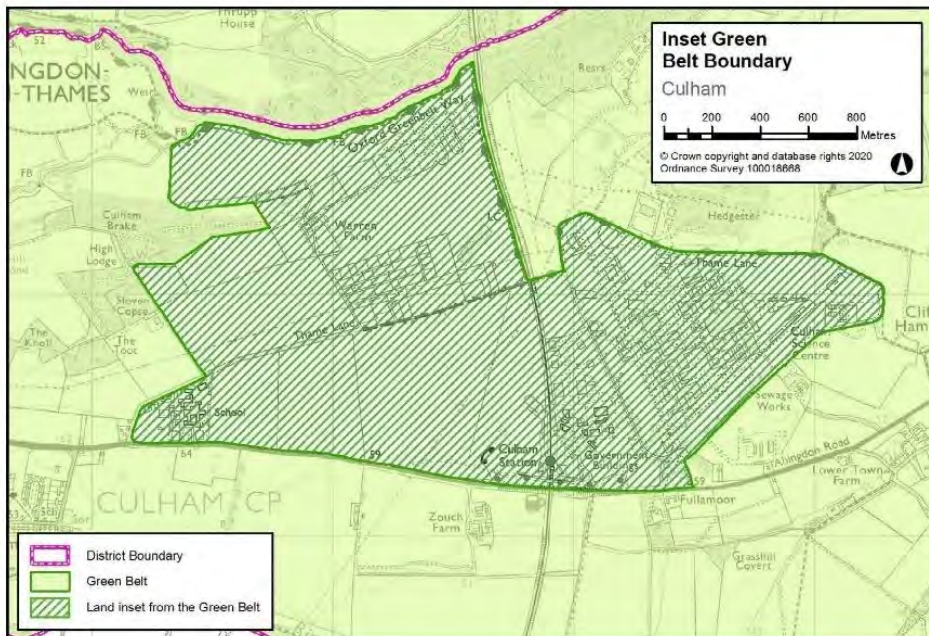
- Sustainable development
- Reducing carbon emissions
- Standards and certifications
- Reducing embodied carbon

# 4. Analysis

## Introduction

### The Parish

Culham is a small village and rural parish in a bend of the River Thames, 1 mile (1.6 km) south of Abingdon in South Oxfordshire with more than 12 centuries of recorded history. The Parish lies entirely within the Green Belt (and the village itself is ‘washed over’), however the recently adopted South Oxfordshire Local Plan has made alterations to the Green Belt to accommodate major strategic allocations in the Parish (see plan below) including safeguarding land for strategic transport schemes being planned within and adjacent to its boundary..



Policy STRAT9 of the Local Plan requires the delivery of 3,500 new homes (extending beyond the plan period), employment, retail and social infrastructure – including a GP surgery and 2FE primary school focussed around the railway station as part of the major strategic allocation in the northern part of the Parish.

There are also a number of other Local Plan designations in the Parish including a mineral safeguarding area, Culham Brake Site of Special Scientific Interest (SSSI) which is located to the north of the major strategic allocation, listed buildings and the Culham Conservation Area.

The main village settlement lies to the south west of the major strategic allocation nestled between Culham Cut, a lock cut to the north of the main stream of the River Thames, and the A415 Abingdon Road. The southern and western areas of the settlement maintains flat, low-lying riverside meadows alongside the parkland characteristics associated with Culham House surrounded by wooded and open farmed hills and valleys reflecting the settlement’s predominately rural character.

### Population

The 2011 Census recorded the population of Culham as 453.

### Economy

Though small, Culham is known internationally through the nearby research centre, Culham Science Centre, home to two major nuclear fusion experiments, JET and MAST.

# 4. Analysis

## Introduction

When JET was built, the European Commission set up the European School in Culham, located to the north east of the main village on the A415, to provide an international education for the children of Euratom scientists who would come to work at JET. The European Commission withdrew from managing the school, and it is now a multicultural multilingual UK state school called the Europa School UK used by Culham village residents. Tourism also plays an important part given Culham's location bordering the Thames.

### Historic development

(Based on the Culham Parish Council and the village run website about Culham and its history)

*“The origins of the parish system go back to Anglo-Saxon times. We do not know when the parish of Culham first came into existence, but a survey of it was made in 940 in the time of King Edmund. The boundaries of the parish seem to be exactly as now, except for the loss of some eyots in the river to Abingdon in 1894. The survey mentions the ford where Abingdon Bridge now stands and refers to 'barrows' (earthworks) at some points along the Parish's eastern boundary; but all trace of the barrows has long since disappeared.*

*The parish of Culham divides geographically into three distinct sections. Most of it lies between Clifton Hampden and a backwater of the Thames once known as Swift Ditch: Andersey Island, comprising the area between the backwater and Abingdon; and the Otneys, an area on the right bank of the Thames adjoining the west side of Sutton Courtenay.*

*The parish is bounded by the Thames to the north, west and south, and by present and former field boundaries to the east. It is low-lying and fairly flat, rising from the Thames floodplain in the south to a north-facing escarpment in the north up to 260 feet (80 m) above sea level. The Thames was certainly navigable during the Middle Ages from London to Henley, and perhaps to Burcot; but the barges moving upstream from Burcot had to face a shallow, rocky bottom at Clifton and a very tricky passage through Sutton to Abingdon. There was, of course, no Clifton or Culham Cut until the 19th Century. At Abingdon the river was again shallow and there were numerous obstructions on the way to Oxford. Hence the wharfage for Abingdon came to be at Culham. We know, for instance, that stone and lead from the dissolved Abbey of Abingdon were brought by road to Culham Wharf to be loaded upon barges for transportation to London. In Tudor times barges became bigger and this made it almost impossible for them to moved between Burcot and Oxford. Hence by two Acts of 1605 and 1624 Parliament set up the Oxford - Burcot Commission to improve the passage of the Thames between these places.*

# 4. Analysis

## Introduction

*The Commission did much to improve the river between 1624 and the outbreak of the Civil War in 1642. It built the first pound locks on the Thames at Iffley, Sandford and Culham. The Culham Lock was constructed about 1636 in a new cutting at the head of Swift Ditch, which was made the main artery for the barge traffic. The remains of the lock can still be seen as well as the assembly pool for barges that lay near it. There was a flash lock about half way along Swift Ditch, which existed at least as early as 1585. Swift Ditch remained the chief navigation channel until 1790 when it was abandoned in favour of the present route through Abingdon. Water communications through Culham were made much easier in 1809 with the construction of The Culham Cut and Lock. The Cut was made partly along the line of the old Speel Ditch, a straggling channel that left the Thames at the head of the present Cut and turned south to rejoin the river near Sutton Mill.*

*Communications by road were poor until the early 15th Century. The main Dorchester - Abingdon road runs through the parish from east to west, but before the reign of Henry V the traveller from Dorchester had to ford the river both at Culham and Abingdon. The highway from Dorchester to Abingdon is undoubtedly very old - it is said in an Act of Parliament of 1416 to have existed from "time immemorial". Between 1416 and 1422 a major scheme for improving communications between Abingdon and Culham was undertaken by the Abingdon Guild of the Holy Cross. Abingdon Bridge, the causeway across Andersey, and the old bridge at Culham were built at the Guild's expense.*



Image Source: SCLHS



Image Source: SCLHS

# 4. Analysis

## Introduction



Image Source: SCLHS

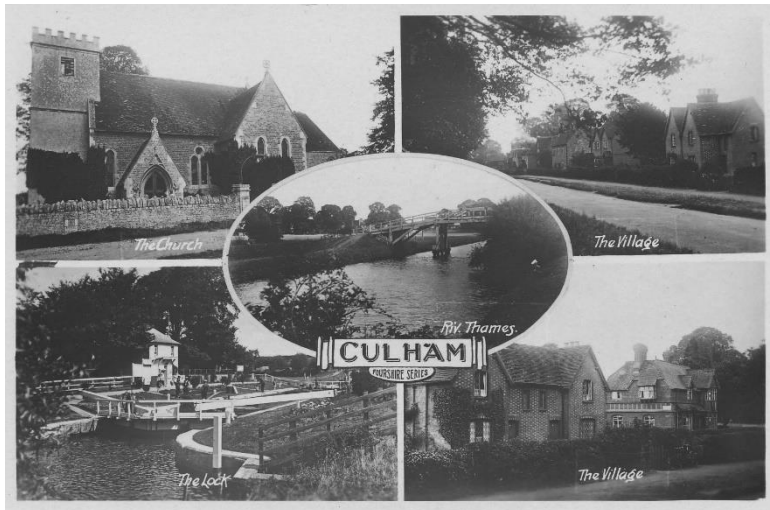


Image Source: SCLHS

*The old bridge is built across the site of the ancient ford known as Culham Hyth; it is of stone and has five perpendicular arches. It lies just to the south of the new bridge erected in 1928 by the Oxfordshire County Council. An Act of Parliament in 1736 created a turnpike trust to maintain the roads between Henley and Abingdon; the trust was empowered to levy tolls for the repair of the roads. Not until 1875 were tolls completely abandoned. The trust set up toll-houses at Culham Bridge and at the junction of Thame Lane with the main highway. The toll-houses are still standing.*

*The highway is joined near the Wagon and Horses Inn by Thame Lane, which used to continue its journey across Clifton Heath. It was cut in 1941 when a Royal Naval Air Station was built on the east side of the railway line between Didcot and Oxford. A field to the north of Thame Lane bounded by the railway line was probably the site of the Abingdon races, held on Culham Heath from the 1730's to 1811. Visitors from Oxford could approach the racecourse by a road, or rather track, from Nuneham.*

*Culham village was never on the main road. The village High Street is part of a long loop beginning at the Wagon and Horses and ending at Culham Bridge. Before 1813 the straight stretch of road from Culham Bridge to the Village green, cutting through Bury Croft, did not exist; the main highway was linked to the village by a road running close to the west side of Culham House. This road was closed when the straight stretch of road to the Bridge was made. Before 1807 a road from the Wagon and Horses ran to the ferry which took travellers over the Thames to Sutton.*

# 4. Analysis

## Introduction

*The ferry lay just to the west of the present bridge. Built in 1807, it was extended over the Culham Cut in 1908. It was privately owned until 1939 when it was jointly purchased by the Berkshire and Oxfordshire County Councils.*

*The railway line from Didcot to Oxford runs through the eastern fringe of the parish. It was built in 1843 and 1844 after the objections of local landowners, the University and the city of Oxford had been overcome. The local station was known as "Abingdon Road" and was served by horse-drawn omnibuses from Abingdon which were timed to meet the trains. When Abingdon secured its own station in 1856 "Abingdon Road" was rechristened "Culham". The old ticket office at Culham Station, a Grade II\* listed building, was designed by the famous Victorian engineer Isambard Kingdom Brunel.*

*Culham's old English name (Cula's Hamm) suggests a possible 6th Century Anglo-Saxon settlement in the bend of the river, and it was a place of some importance in later Saxon times. For six centuries it was a possession of the Abbey of Abingdon, though the Abbey did not have continuous possession before the middle of the 10th Century; and it was 150 years after that before the Abbey finally secured Andersey. The Mercian King Offa (d.796) is said to have had a hunting lodge on Andersey. The remainder of the parish was apparently in royal hands at this time. The abbey later claimed that King Kenwulf of Mercia (796-821) had granted Culham to it and produced two charters, dated 811 and 821 to prove its case.*

*The charters are certainly spurious, but may nonetheless have a basis of truth. The forgery of documents by monks was a not unusual procedure in the Dark Ages; they probably forged them to ensure their Abbey's possessions had a legal basis. This may well be the case with Culham. Certainly, Culham enjoyed a spell of royal favour in the Middle Ages.*

*The manor of Culham remained in the hands of Abingdon Abbey as a rest house until the dissolution of the Abbey in 1538 when it was seized by the Crown. In 1545 Henry VIII granted it to a London wool merchant, William Bury, in exchange for land in the Isle of Sheppey and £600. The house is largely of fifteenth century origin but in 1610 Thomas Bury rebuilt the north front. Bury's house was much larger than the present one, for an eastern section was demolished during the Civil War. The Manor House was in possession of the Bisshopp family from 1666 until 1856 but their interest in it ceased in 1749 and the Manor began a long period of decline; for many years it was a farm house. However, the house was restored splendidly by Sir Esmond Over from its sadly dilapidated state of 1933.*

*The majority of villagers obtained a living from agriculture, farming strips of land in the great open fields which surrounded the village. Originally, there were two vast arable fields, perhaps even as late as 1539; for a survey of that year speaks only of Town and Contard Fields. By the middle of the 17th Century there were three fields (Ham, Middle and Contard); during the 18th century there was a change to a four field system.*

# 4. Analysis

## Introduction

*The enclosure Award of 1813 mentions four fields: 1. Contard - forming a triangle between the main highway and Thame Lane and ending in the east at Culham Heath, 2. Ham - south of the main highway, from the Clifton boundary to a point perhaps half way between the Wagon and Horses and the boundary, 3. South Middle Field - the remainder of the arable area south of the main highway, 4. North Middle Field - mostly north of the main highway between the Wagon and Horses and Culham Bridge, and also north of Thame Lane for a short distance at its western end.*

*The exact boundaries of the fields are hard to trace. They comprised altogether some 700 acres. Apart from the arable land there were before enclosure a number of hedged meadows and pastures: these were to be found on Andersey, on both sides of Swift Ditch and in the north of the parish. In addition, Culham Heath was a large tract of land in the north east of the parish south of Nuneham Park and reaching in places the main Abingdon-Dorchester Road. After enclosure much of the heath was drained and brought under cultivation. The 18th century saw the appearance of large farms. Tye, Warren and the Manor Farm were the best known. There were about 40 Houses in the village at this time. The houses lay mostly north and south of the main village street, i.e. the present High Street, though for most of its length the old street was farther north, i.e. nearer to Culham House, than the present High Street. The alteration to the present line was made between 1810 and 1813 at the time of enclosure when the road across Bury Croft was constructed.*

*Most of the village was rebuilt in 1869 and 1870 and consequently few of the old dwellings survive. Indeed, the only old cottage still in existence is the village store, of 17th century origin and refronted in the 18th century. Not even the inns can claim much antiquity. The parish now has three: the Wagon and Horses, the Lion and the Jolly Porter (formerly the Railway Hotel). The Wagon and Horses can be traced back to 1795, though the building is early 19th century; the Lion (formerly the Sow and Pigs) is a fairly modern building, but it too can be traced back to 1795; the Jolly Porter was built about 1846. In the late 18th century there were half a dozen malshouses in the village.*



Image Source: SCLHS

# 4. Analysis

## Introduction

*Culham's oldest Building is the Manor House, originally a medieval grange of the Abbots of Abingdon. The house is largely of 15th century date, but in 1610 Thomas Bury rebuilt the north front. Bury's house was much larger than the present one, for an eastern section was demolished during or after the Civil Wars. There is still a room within the house called the Abbot's Chamber which once had heraldic glass depicting the arms of Abbot Coventry, who died in 1512. In the grounds is a dovecote, dated 1685, and bearing the initials of Sir Cecil Bisshopp. It is believed to be one of the three largest in England. When the Bisshopps ceased to bother with Culham, the Manor House began a long period of decline; for many years it was a farmhouse.*



Image Source: SCLHS



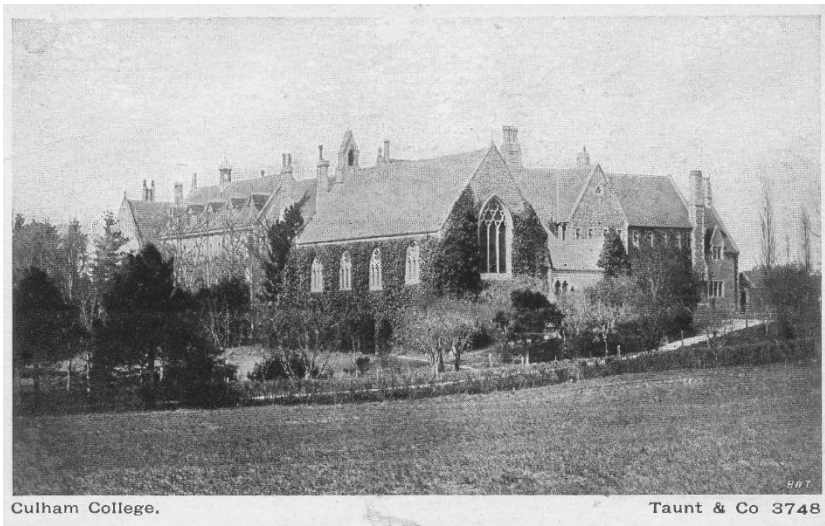
Image Source: SCLHS

*The largest house in the village is Culham House, built about 1775 by John Phillips, lay rector of the parish. Phillips was a London builder. His ancestors hailed from Hagbourne and became master carpenters to George I and George II. The Phillips family first appeared in Culham about 1736 and were here until 1935. As lay rectors they were entitled to sit in the chancel of the church and were also legally responsible for the chancel's upkeep. Several memorials to members of the family are in the church. John Phillips erected a handsome redbrick building of five bays, with contemporary staircase, overmantles and doorcase. The house was enlarged about 25 years later to seven bays. It was once noted for its collection of china.*

# 4. Analysis

## Introduction

*The old Vicarage was built about 1758, probably by Benjamin Kennicott, vicar of Culham 1753-83. It was enlarged by a later vicar, Robert Walker, in 1849. It has now been sold by the church authorities. The only large building beyond the confines of the village is Culham College of Education. The building, erected in 1852 was designed by Joseph Clarke, a minor architect of the Victorian era. Clarke designed the College in the neo-Gothic style which was fashionable at the time. The tower block was opened in 1973 when Teacher Training Colleges were being expanded. Europa School was established in 2012 and took over the school site and pupils fully from the Culham European School in 2017. The European School was located on the site from 1975, in buildings which had previously been the home of Culham College, a Church of England teacher training college from 1853.*



Culham College.

Taunt & Co 3748

Image Source: SCLHS

*There is no sign of any school in the parish before the early 19th century. In 1808 younger children learned to read and write in two small schools, presumably held in cottages; in 1815 a Sunday School was started, its master being paid from the rates. Nevertheless, provision for education was very unsatisfactory until 1850, when the village Church of England School was erected at a cost of £438. Some additions to the premises were made in 1897. Usually a mixed all-age school, it was reorganised in 1924 for infants and girls only, but in 1931 the senior girls were transferred to Dorchester. Temporarily closed in 1948, the school was re-opened in 1951.*

*St Paul's Church is situated at the end of the village green, opposite the Manor House and gardens. It has a long history and was closely linked in medieval times to Abingdon Abbey. Rebuilt in Victorian times, replacing one of late twelfth century or early thirteenth century origin; the tower is its oldest part, dating back to 1710. The Mediaeval Church was about the same length as the present building but had a narrower nave, In 1852 the mediaeval nave was beyond repair and was rebuilt; the cost was borne partly by a parish rate and partly by donations. The chancel was rebuilt at the expense of the lay rector, John Shawe Phillips. A new parish cemetery bordering the existing churchyard was consecrated by the Bishop of Dorchester in 2004 and the Parochial Church Council received approval from the Home Office for the closure of the churchyard, which is full.*

# 4. Analysis

## Introduction



Image Source: SCLHS

*The parish is also host to the Culham Brake Site of Special Scientific Interest (SSSI) on the northern edge of the parish boundary, a small area of wet willow woodland with large sedges and wetland wildflowers on the ground. The site is particularly important for the presence of a large population of the nationally scarce Lodden Lily. Other wetland wildflowers found here include meadowsweet, valerian and yellow flag iris.*

*Culham, in the course of its known history of more than 1,000 years, has seen many changes in the evolution of England and many changes within its own boundaries. Yet it remains a unity despite the economic and social pressures of the 20th century.”*

# 4. Analysis

## Settlements and Designations

### Settlement pattern

The Saxon settlement of Culham was developed just above the Thames floodplain taking advantage of the higher ground and proximity to well-watered river meadows. The small village of Culham was once much larger and was situated to the west of the existing village settlement. The Manor House and St. Paul's Church are the only visible remains of the medieval settlement.

The historic village was established through a dispersed pattern around these remaining buildings, including Culham House, with later gradual linear development concentrated along the High Street (including the cul-de-sac development of The Glebe), Tollgate Road, and Thame Lane.

The linear development of the village continued the open and spacious feel of the historic dispersed pattern of development through either pairs of semi-detached homes or detached homes, set back along a continuous building line with grass verges, front gardens and low level hard boundary treatments. Mature trees and hedgerows throughout the settlement contribute to the parkland characteristics that dominates the area surrounding Culham House.

The Glebe cul-de-sac development follows a similar pattern of semi-detached homes considerable set back from the street, with a continuous building line and large grass verges creating a sense of openness along the street.

Development on Tollgate Road continues the set back building line from the street with front gardens and includes some bungalows. There is no grass verge and some of the front gardens here have been covered by hardstanding. This coupled with the pavement and curbs, gives Tollgate Road a more formalised, suburban character, although the open farmed hills and valleys landscape opposite the built development and mature hedgerows and planting further along the road maintains a rural character. The former Waggon and Horses dating back to the 19<sup>th</sup> century is located in a prominent position at the entrance to the village where Tollgate Road meets the A415. It is currently boarded up and has become an eyesore in this prominent location.

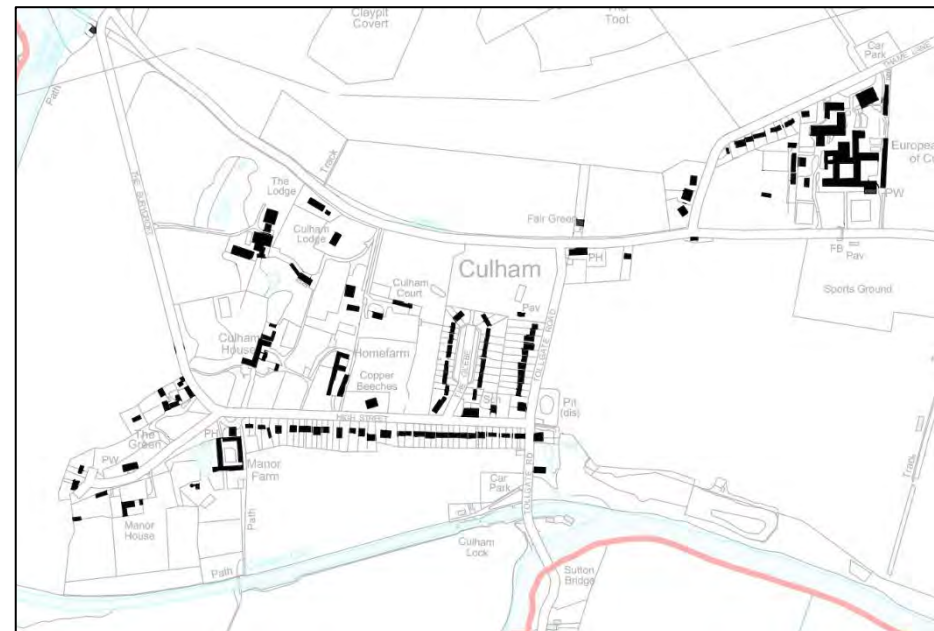


Figure ground diagram of Culham village and Europa school complex

# 4. Analysis

## Settlements and Designations

### Local materials

**Walls:** The majority of the buildings along the High Street has adopted some or all of the elements established by nos. 7-11 The Green, including the use of predominantly red brick, flemish bond with glazed headers, brick feature lintels and quoins.

There remains three excellent arts and crafts buildings in the village, The Lion (former PH) on the High Street sharing a striking resemblance with 22-23 High Street (a pair of symmetrical, semi-detached houses) all dating back to the 19<sup>th</sup> century and rich in materials and architectural detail including scalloped tile hanging, English bond brickwork, timber frame filled with a combination of straight and herringbone pattern brick.

English and common bond brickwork, tile hanging and brick feature lintels features again at the School House erected in 1850 at the entrance to The Glebe. The use of red brick, including brick feature lintels, is common in later additions at The Glebe and on Tollgate Road. The prominent former Waggon and Horses PH features a mix of stone and rendered walls with red brick quoins.

Outside of the main village settlement of particular note includes Station House near the railway station, with the use of red brick with English bond brickwork, and Tollgate Cottage at Culham Old Bridge, with the use of red brick, flemish bond with glazed headers.



# 4. Analysis

## Settlements and Designations

### Local materials

**Roofs:** The use of plain clay tiles is common throughout the settlement.

Ridge mounted, gable end and centred, chimney stacks are a prominent and distinctive feature of roofscapes in the village although there are some located further down the roof slope.

The arts and crafts buildings include decorative barge boards, pattern crested ridge tiles and ornate brickwork detailing to the chimneys. The School House and prominent former Waggon and Horses PH also includes ornate brickwork detailing to the chimneys.

**Plot Boundary:** The majority of buildings are set behind grass verges and front gardens creating an open spacious feel.

Low level brick walls and hedges, with the exception of the larger historic buildings in the village where these treatments are high level, are common in front boundary treatments with the occasional use of timber fencing (picket and closeboard).

Mature trees in front gardens are also very common reflecting Culham's sense of greenery and spaciousness.



# 4. Analysis

## Settlements and Designations

### Archeology

Archaeology in South Oxfordshire is looked after centrally across the whole county by Oxfordshire County Council who will be consulted as per the South Oxfordshire Design Guide. The [Aerial Archaeology Mapping Explorer](#) created by Historic England illustrates the extensive amount of archaeology that has been identified in Culham.

### Listed buildings

There are 17 listed buildings or structures in Culham.

- Culham Station Ticket Office and Waiting Room
- Dovecote west of Culham Manor
- Culham Manor
- Culham Old Bridge

are all Grade II\* listed buildings or structures. The remaining buildings or structures are Grade II listed:

- Pound lock on Swift Ditch
- Culham Court
- 36 and 37 High Street
- Church of St Paul
- The Maud Hales Bridge (that part in Culham parish)
- Schola Europa
- Culham House
- 13 The Green

- Sutton Bridge and Causeways (that part in Culham parish)
- Culham Manor
- Sundial north east of Culham Manor
- Culham Old Bridge
- Bridge over Culham Cut
- Culham Station Overbridge
- Thame Lane Bridge
- A small part of the Grade II listed Sutton Courtenay Manor Park and Garden is located in the Parish.

### Other historic environment designated heritage assets

There are 3 Scheduled Ancient Monuments in Culham.

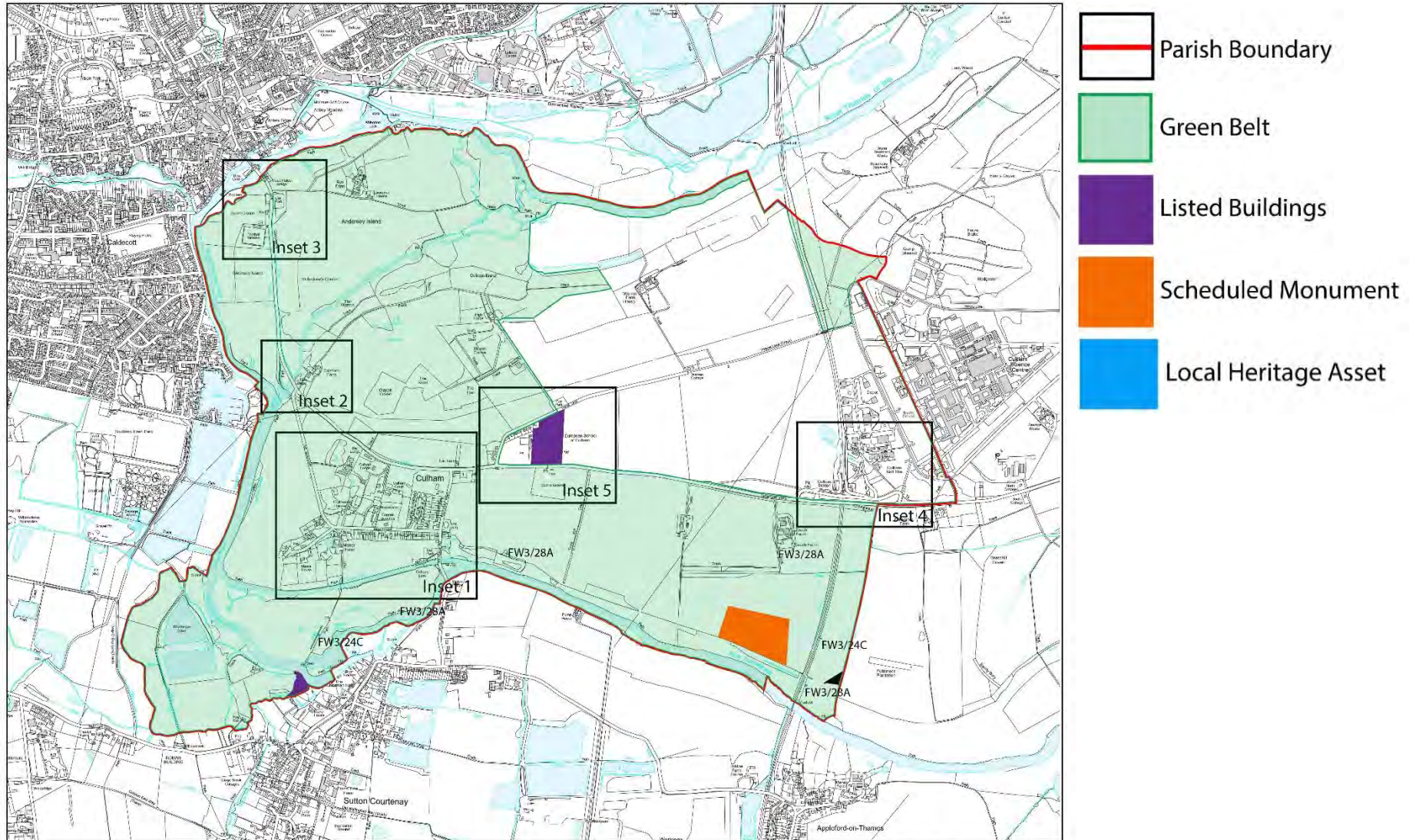
- A settlement site north of the Thames
- Culham Bridge
- Dovecote at Culham Manor
- A small part of the round barrow cemetery at Fullamoor Plantation is located in the Parish.
- Part of The Maud Hale's Bridge is located in the Parish.

### Conservation Area

Culham Conservation Area was designated on 11 December 1984. Unfortunately, all the South Oxfordshire District Council records concerning the Culham Conservation Area were lost in the 2015 fire. As there is no conservation area appraisal the following maps identify the location of listed buildings and structures, scheduled ancient monuments, special features in the Conservation Area, as well as the Conservation Area boundary, and locally important buildings.

# 4. Analysis

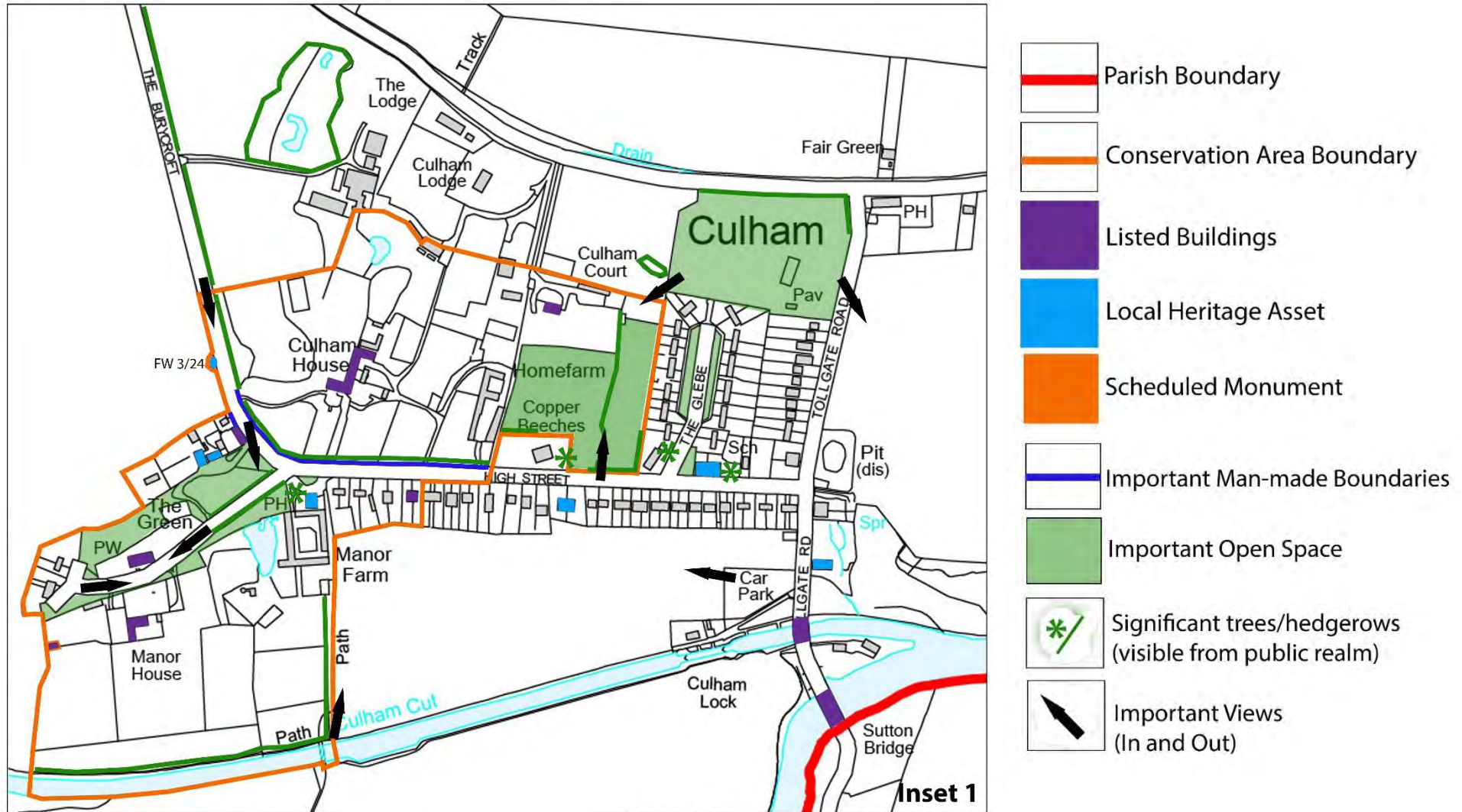
## Settlements and Designations



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# 4. Analysis

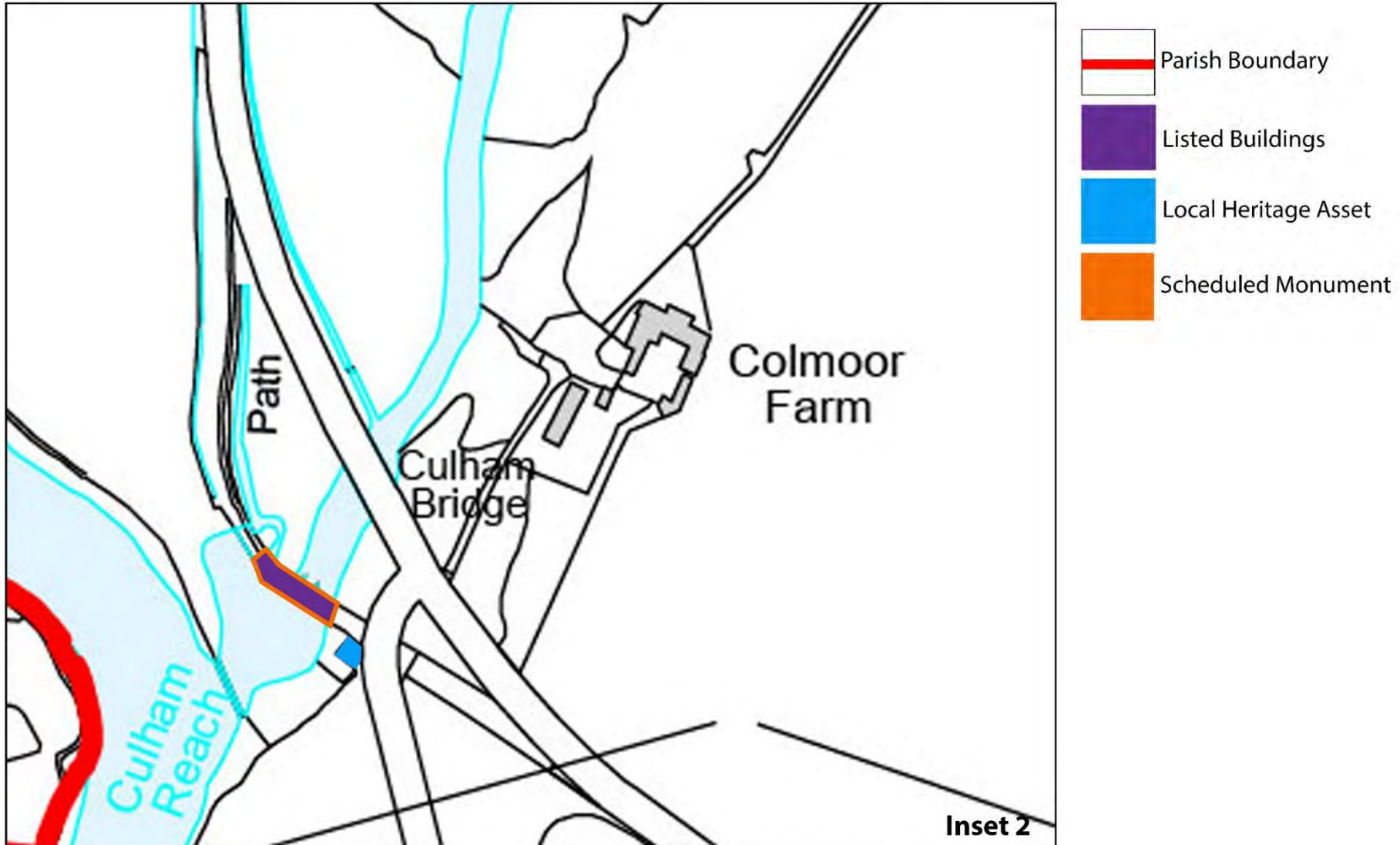
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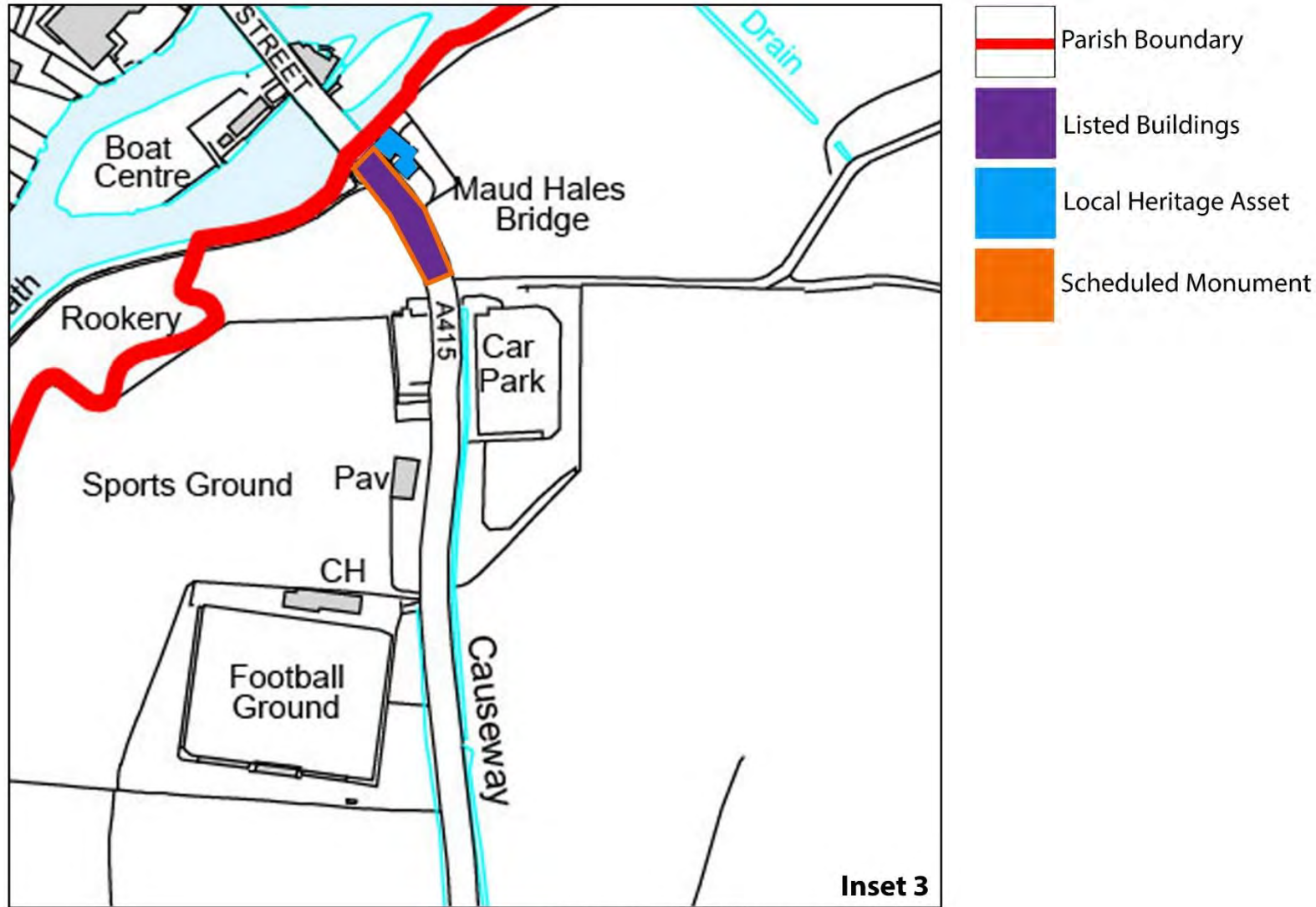
## Settlements and Designations



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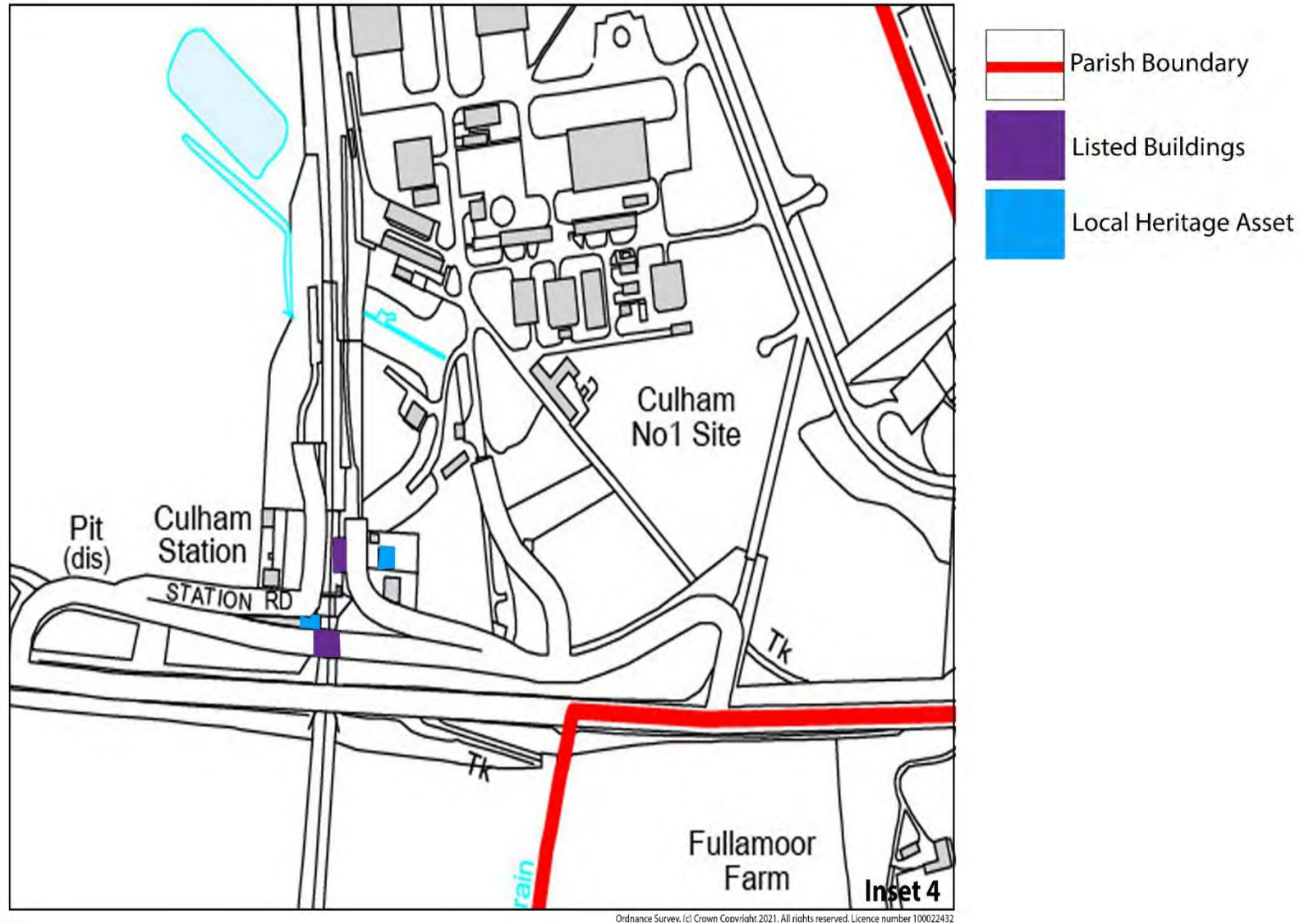
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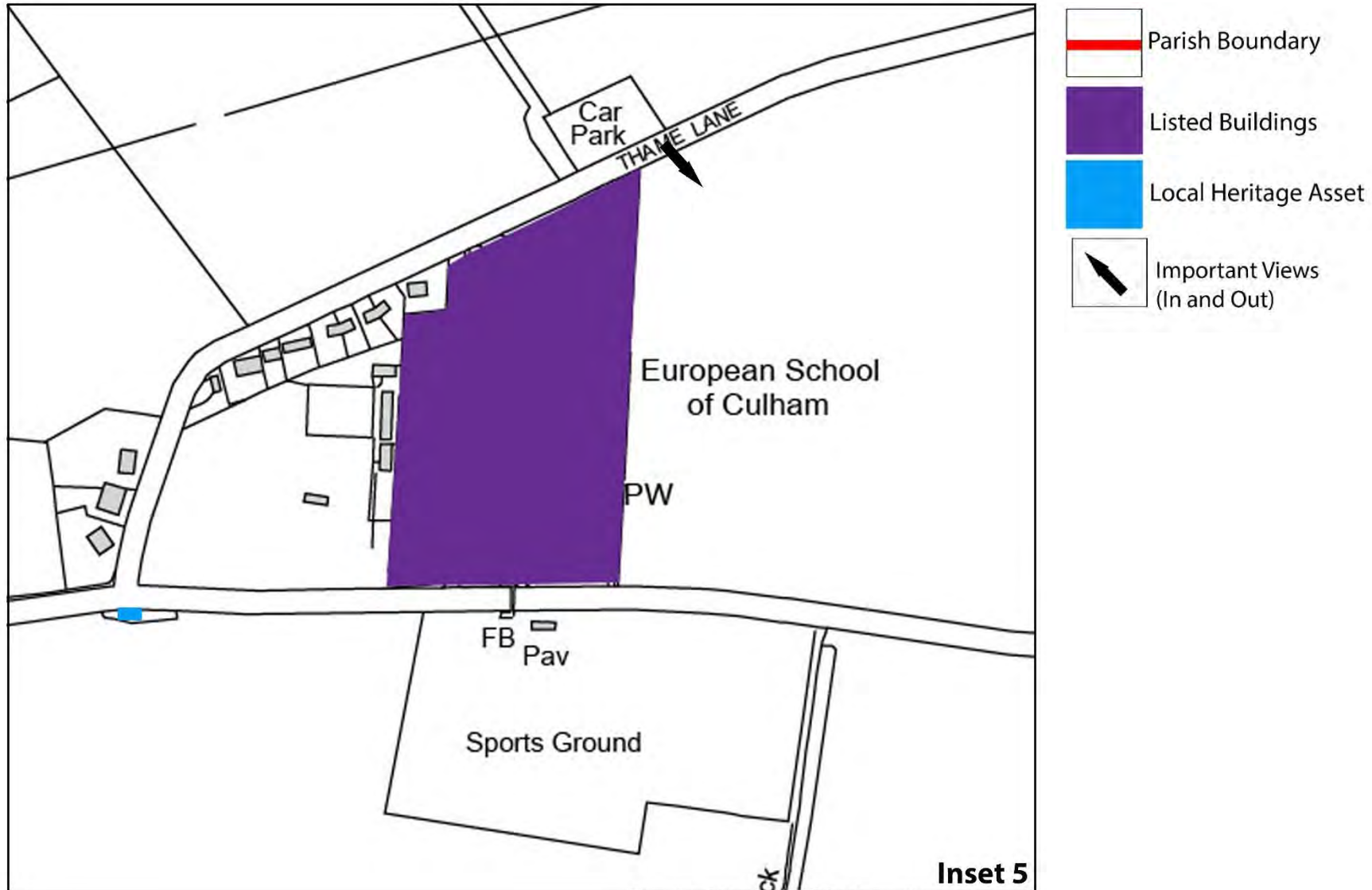
# 4. Analysis

## Settlements and Designations



# 4. Analysis

## Settlements and Designations



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# 4. Analysis

## Settlements and Designations

### Natural environment

The parish is host to the Culham Brake Site of Special Scientific Interest (SSSI) on the northern edge of the parish boundary, a small area of wet willow woodland with large sedges and wetland wildflowers on the ground. The site is particularly important for the presence of a large population of the nationally scarce Lodden Lily. Other wetland wildflowers found here include meadowsweet, valerian and yellow flag iris. Alongside the Culham Brake SSSI, there is a variety of other priority habitat areas in the Parish. The village itself hosts Traditional Orchard habitats as well as Deciduous Woodland. The Culham Neighbourhood Plan has defined a network green and blue infrastructure assets in the Parish as a means of providing environmental support for the community and wildlife.

# 4. Analysis

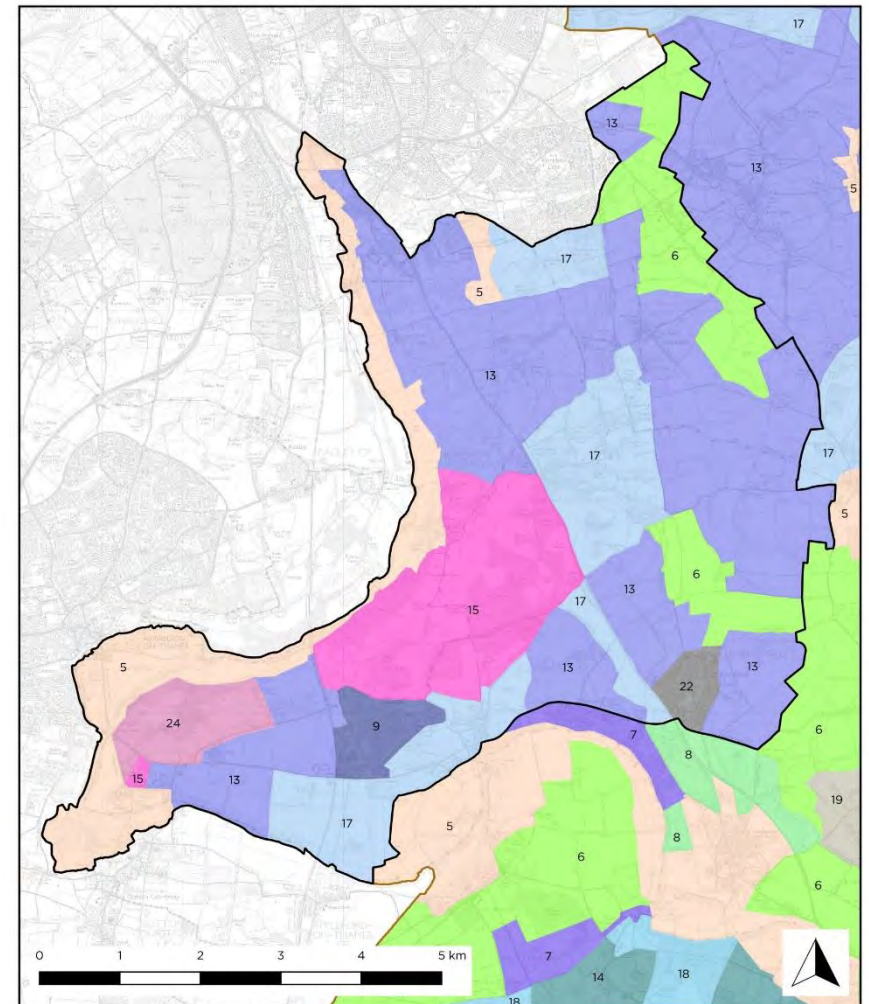
## Landscape Character

### Landscape character

“At its lowest point, in the south east, the parish is 159 feet above sea level, but almost immediately the land rises sharply to 175 feet, thus forming an escarpment along the river bank. Just east of the backwater the ground rises steadily to form Culham Hill, which at its peak is 250 feet above sea level. From the top of the hill the land descends once more until it meets the Thames again 170 feet above sea level” ([British History Online](#)).

The South Oxfordshire Landscape Assessment identifies Culham as lying within the Nuneham Courtenay Ridge Landscape Character Area which the South Oxfordshire Design Guide does not differentiate from the Oxford Heights Landscape Character Area.

In the wider context the “landform rises eastwards from the River Thames, creating a dramatic ridge with views over the river towards Abingdon. The northern extent of the ridge to the west of Nuneham Cortenay stretches southwards to Culham Bridge” ([SODC LCA, 2017](#)).



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Ordnance Survey 100018668 South Oxfordshire District Council

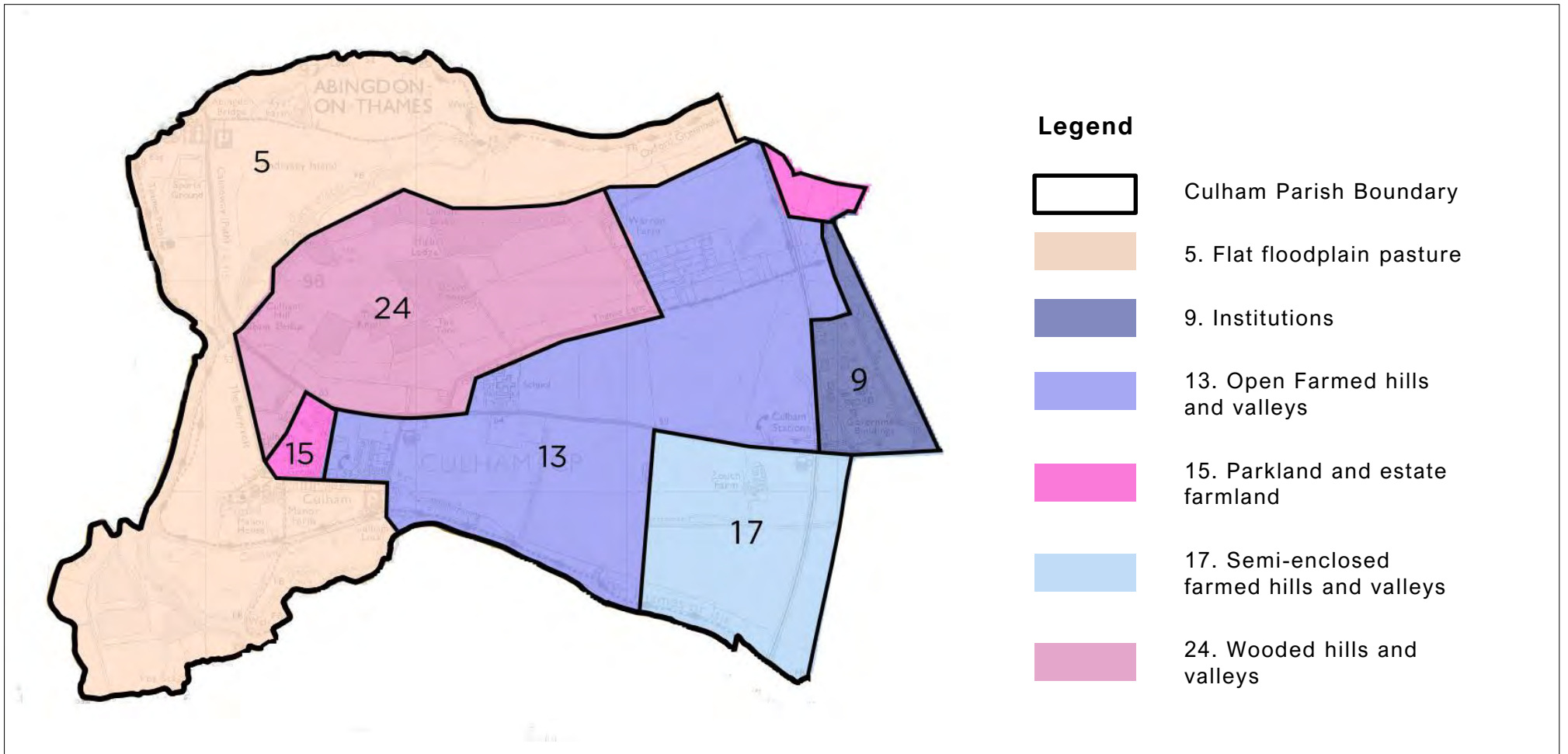
| Legend                              |                                   |  | South Oxfordshire<br>Landscape<br>Character Assessment<br>Map: Nuneham<br>Courtenay Ridge (LCA<br>2) LT<br>Scale: 1:52,000<br>Date: 23.08.17 |  |
|-------------------------------------|-----------------------------------|--|--|--|
| Nuneham Courtenay Ridge (LCA 2)     | 8. Floodplain wetland             | 17. Semi-enclosed farmed hills and valleys |  |  |
| South Oxfordshire District Boundary | 9. Institutions                   | 18. Semi-enclosed rolling downs            |  |  |
| 1. Airfields/MOD sites              | 10. Minerals / landfill sites     | 19. Undulating open vale                   |  |  |
| 2. Amenity landscapes               | 11. Open dipslope                 | 20. Undulating semi-enclosed vale          |  |  |
| 3. Commons and heaths               | 12. Open escarpment               | 21. Undulating wooded vale                 |  |  |
| 4. Enclosed escarpment              | 13. Open farmed hills and valleys | 22. Urban areas                            |  |  |
| 5. Flat floodplain pasture          | 14. Open rolling downs            | 23. Wooded dipslope                        |  |  |
| 6. Flat open farmland               | 15. Parkland and estate farmland  | 24. Wooded hills and valleys               |  |  |
| 7. Flat, semi-enclosed farmland     | 16. Semi-enclosed dipslope        |  |  |  |

Landscape types in the Nuneham Courtenay Ridge Landscape Character Area, Source: [SODC LCA, 2017](#)

# 4. Analysis

## Landscape Character

Culham Parish Landscape Types Source: [SODC LCA, 2017](#)



# 4. Analysis

## Landscape Character

| LANDSCAPE TYPE                    | KEY CHARACTERISTICS  |
|-----------------------------------|--|
| 5. Flat floodplain pasture        | <ul style="list-style-type: none"> <li>• Flat, low-lying riverside meadows alongside the River Thames, typically dominated by permanent pasture with a distinctively ‘wet’, riparian character.</li> <li>• Prone to flooding with distinctive network of drainage ditches.</li> <li>• Comparatively strong landscape structure with willows conspicuous along the riverside.</li> <li>• Intimate and pastoral character.</li> <li>• Generally low intervisibility, although views along the valley may be possible in some more sparsely vegetated areas.</li> <li>• Comparative inaccessibility creates a tranquil, remote character with only localised intrusion close to main urban area of Abingdon.</li> </ul>               |
| 9. Institutions                   | <p>Culham Laboratories is located within this area and comprises a complex of institutional buildings within landscaped grounds.</p> <ul style="list-style-type: none"> <li>• Landscaped setting with mature trees and semblance of parkland character but lacking its formal features.</li> <li>• Dispersed complex of buildings, signs and land uses have an urbanising influence on rural context of the site.</li> </ul>   |
| 13. Open farmed hills and valleys | <ul style="list-style-type: none"> <li>• Rolling plateau landform.</li> <li>• Large-scale farmland, mostly in arable cultivation.</li> <li>• Large fields, with rectilinear field boundaries, typical of parliamentary enclosures.</li> <li>• Weak structure of tightly clipped or gappy hedgerows, with few hedgerow trees.</li> <li>• Open, denuded and exposed character, with prominent skylines and hillsides and high intervisibility.</li> <li>• Distinctive elevated and expansive character on ridges and higher ground, with dominant sky and long views.</li> <li>• Predominantly rural character but some localised intrusion of main roads (such as the A415), overhead power lines and built development.</li> </ul> |

Culham Parish Landscape Types Source: [SODC LCA, 2017](#)

# 4. Analysis

## Landscape Character

| LANDSCAPE TYPE                             | KEY CHARACTERISTICS   |
|--|---|
| 15. Parkland and estate farmland           | <p>Small scale area with parkland characteristics associated with Culham House.</p> <ul style="list-style-type: none"> <li>• Well-managed parkland character with formal features such as avenues and free-standing mature trees in pasture, clumps and blocks of woodland, exotic tree species, formal structures and boundary features.</li> </ul>  |
| 17. Semi-enclosed farmed hills and valleys | <ul style="list-style-type: none"> <li>• As per 13., though with a stronger structure of hedgerows and trees which provide clearer definition of field pattern.</li> <li>• Predominantly intensive arable land use and rural character.</li> <li>• Landform and landscape structure create enclosure and reduce intervisibility.</li> </ul>   |
| 24. Wooded hills and valleys               | <ul style="list-style-type: none"> <li>• Similar to no.17 but with a particularly strong structure of hedgerows, trees and woodlands at the western end of the greensand plateau and steep escarpments of the River Thames.</li> <li>• Strong relief, mixed land use and blocks of woodland create an attractively diverse landscape.</li> <li>• Intervisibility reduced by landform and landscape structure to create a more enclosed and intimate landscape, but long views possible from hillsides and higher ground across Thames valley.</li> <li>• Predominantly rural character with few detracting influences.</li> </ul> |

Culham Parish Landscape Types Source: [SODC LCA, 2017](#)

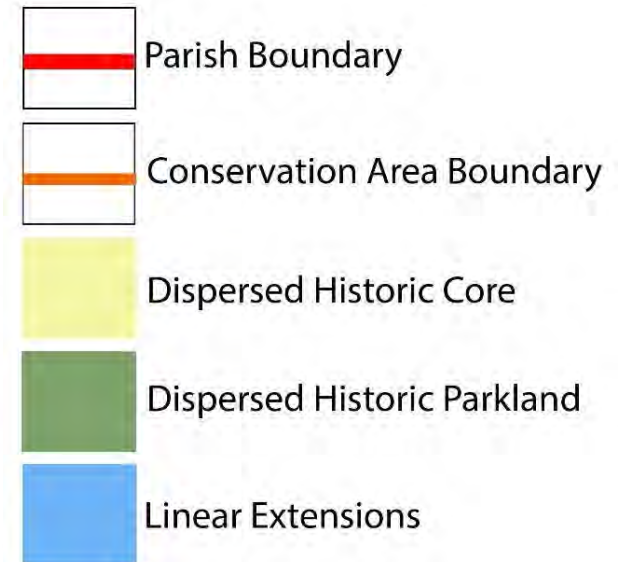
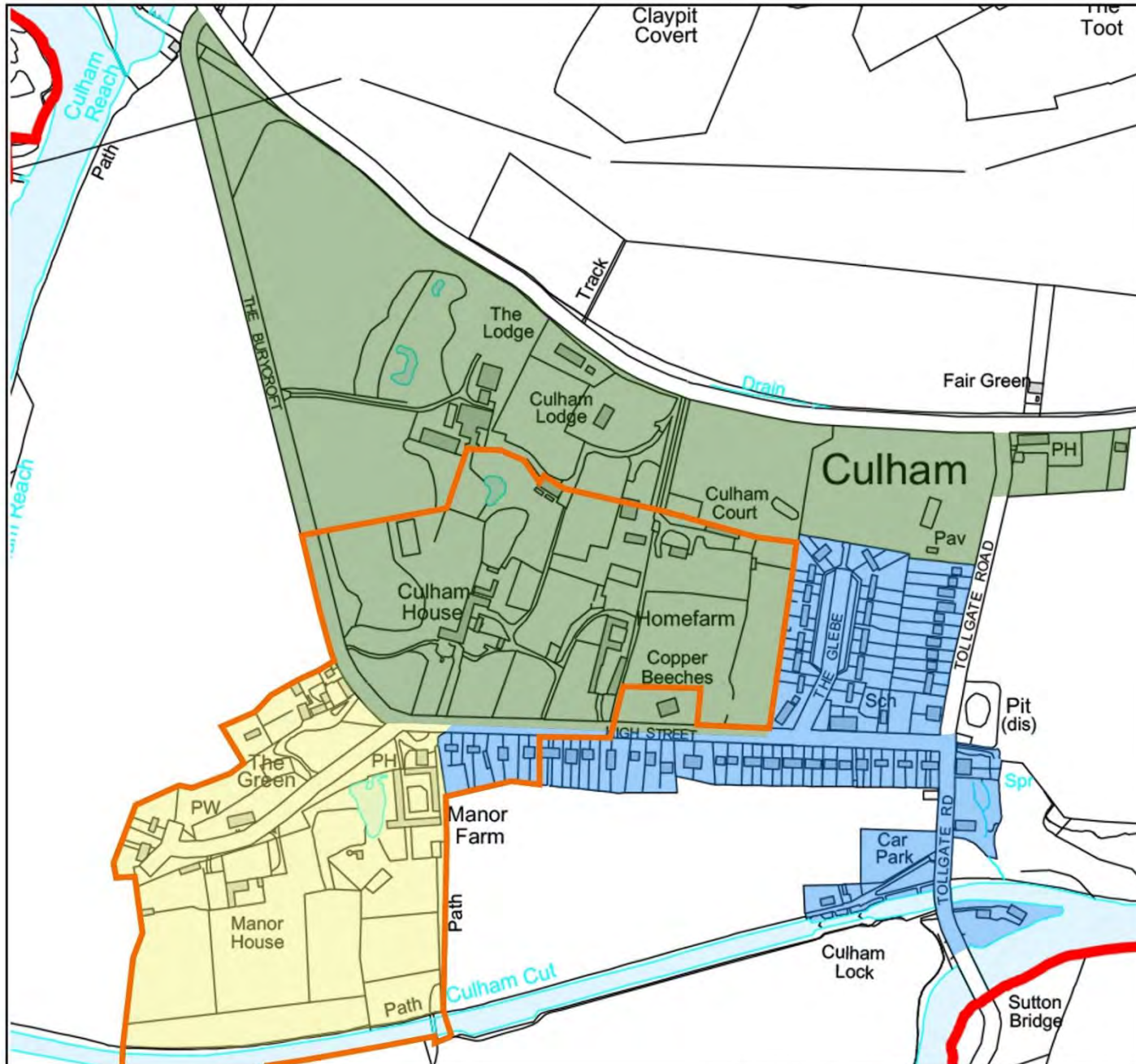
## 5. Design Codes

The Code establishes the principles of essential design considerations in the residential environment of the main village: dwelling design, boundary design, building materials and landscaping, based on the analysis of local character presented in this report, community consultations and discussions with members of the neighbourhood plan steering group. Beyond these considerations, there remain other design matters where the Code does not need to be prescriptive as there is variation in the existing character.

For the purposes of the Code, the main village settlement has been divided into three main character areas: its dispersed historic core, dispersed historic parkland and its linear extensions (see Plan overleaf). Outside the main village settlement there are also important characteristics which the Code has addressed.

For each area, including the area outside the main village settlement, the Code translates the standards into specific requirements. For ease of reference, the Code numbering matches each area's Code to the relevant section in the Joint Design Guide e.g. Place and Setting 1.0 – 1.9; Built Form 5.0 – 5.14 etc. Throughout the Code, there are local photographs to illustrate the guidance where necessary.

# Culham Character Areas



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# 5. Design Codes

## Dispersed Historic Core

| Place and Setting |  |
|-------------------|--|
|                   | <i>Joint Design Guide: “Ensure: A contextual analysis including an opportunities and constraints plan (which will inform your design rationale) of the wider and immediate site context has been prepared.”</i>  |
| 1.0               | <i>A contextual analysis should identify existing networks of natural features, including watercourses, trees, woodland, hedgerows, green spaces, field patterns, habitats and public rights of way (footpaths, bridleways, etc.)</i>  |
| <b>DHC1.0.1</b>   | Proposals should acknowledge the Village Green and its Significant Trees and Hedgerows (identified in this Code Analysis), including the small pond adjacent to the Church of St Paul and the larger pond at the Manor House and the priority habitat of traditional orchard within its grounds, as a valuable historic setting in the character and appearance of the Conservation Area contributing to the tranquillity of rural life and as a haven for wildlife. |
| 1.1               | <i>A contextual analysis should identify the landscape character, natural features and topography highlighting visually prominent areas</i>  |
| <b>DHC1.1.1</b>   | Proposals should acknowledge the key characteristics of the Flat Floodplain Pasture landscape type in the Nuneham Courtenay Ridge Character Area identified in this Code Analysis.   |
| 1.2               | <i>A contextual analysis should identify attractive and/or sensitive views (both of and from built and natural features) into, out of and within the site</i>  |
| <b>DHC1.2.1</b>   | Proposals should acknowledge the variety of attractive internal views on the winding roads of The Burycroft and The Green (identified in this Code Analysis).  |
| <b>DHC1.2.2</b>   | Proposals should acknowledge the way in which most plots and buildings are hidden by surrounding tree cover with long distance views restricted by tree cover in most parts, reinforcing the attractive skyline of this character area visible from Culham Lock Car Park and along Culham Cut (identified in this Code Analysis).  |
| <b>DHC1.2.3</b>   | Proposals should acknowledge the ‘secretive’ quality of the Grade II listed Church of St Paul and the Grade II* listed Culham Manor which is largely hidden from the public realm by trees and other well-established vegetation ‘guarding’ the approach to it from the village and avoid harming the Important View from The Green where both buildings are revealed (identified in this Code Analysis).  |

# 5. Design Codes

## Dispersed Historic Core



**The Lion, High Street**

Former public house comprising of a two storey detached arts and craft building dating back to the 19<sup>th</sup> century converted to a single dwelling in 2008. Associated with the Morrell’s Brewery who once owned all 39 cottages in the village. A prominent building constructed of high quality materials possessing visual detail and interest which contributes positively to the character of the Conservation Area.



**Nos. 7 -11 The Green**

Part of the rebuilding of the village in in 1869 and 1870, The majority of the buildings along the High Street has adopted some or all of the elements established by nos. 7- 11 The Green, including the use of predominantly red brick, flemish bond with glazed headers, brick feature lintels and quoins, plain clay roof tiles, and ridge mounted gable end chimney stacks.

| Place and Setting |   |
|-------------------|---|
| 1.3               | <i>A contextual analysis should identify buildings and structures of historical importance including listed buildings, associated setting and historic views, historic landscape pattern and features (historic landscape character), conservation areas, historic parks and gardens and archaeological remains</i>   |
| <b>DHC1.3.1</b>   | Proposals should retain the built form and architectural features of The Lion on the High Street as a local heritage asset and should acknowledge its prominent location in attractive internal views and its positive contribution to the character and significance of the Conservation Area (as identified in this Code Analysis).   |
| <b>DHC1.3.2</b>   | Proposals should retain and enhance the built form and architectural features of nos. 7 -11 The Green as local heritage assets and should acknowledge the prominent role the buildings play in setting a pattern for new buildings which defines the character of the local area and their positive contribution to the character and significance of the Conservation Area.  |
| <b>DHC1.3.3</b>   | Proposals should acknowledge the special interest of this part of the Culham Conservation Area as highlighted in the Design Codes, including the following characteristics: <ol style="list-style-type: none"> <li>a. The origins of the main village settlement as a typical Saxon settlement developed just above the Thames floodplain taking advantage of the higher ground and proximity to well-watered river meadows;</li> </ol> |

# 5. Design Codes

## Dispersed Historic Core

| Place and Setting |  |
|-------------------|--|
| DHC1.3.3 (cont)   | <ul style="list-style-type: none"> <li>b. The prominence of the Grade II listed no. 13 The Green as the only old cottage still in existence surviving the rebuilding of most of the village in 1869 and 1870;</li> <li>c. Other significant buildings, including the Grade II listed Church of St Paul and the Grade II* listed Culham Manor and listed structures within its grounds, both of which is less immediately visible from the road, contribute to the character and appearance of the Conservation Area;</li> <li>d. Important Open Spaces include the churchyard and the grounds of Culham Manor in addition to the Village Green identified in this Code Analysis;</li> <li>e. The attractive views along the village streets and lanes in this part of the Conservation Area identified in this Code Analysis;</li> <li>f. Fine groups of mature trees along The Green providing an important sense of arrival at Culham Manor and the Church of St Paul and on the western part of the Village Green north of the Church providing an important sense of enclosure;</li> <li>g. The tranquillity of this part of the Conservation Area is enhanced by the presence of trees and mature vegetation, framing the green space of the irregular shaped Village Green. With the exception of the Burycroft as the main road, there are no pavements or street lights;</li> <li>h. The rural setting and character of this part of the Conservation Area is in danger of being spoiled by light pollution from the adjoining larger centres of Abingdon and Didcot and proposed future growth proposals to the north of the Parish.</li> </ul> |
| 1.6               | <i>A contextual analysis should settlement structure of the site and surrounding area: this includes studying the historical development of the settlement, its townscape; structure and hierarchy of streets, spaces, facilities, existing connections (including footpaths and cycle routes), gateways, nodes, density, plot and block sizes. Figure ground diagrams can help explain a settlement structure.</i>  |
| DHC1.6.1          | Infill developments will be required to demonstrate that proposed buildings on infill plots will reinforce local distinctiveness through incorporating open spaces which are characteristic of the dispersed settlement pattern to avoid reducing the open character of the area.  |
| 1.8               | <i>A contextual analysis should identify the streets and public spaces surrounding the site, the enclosure of streets and public open spaces, the layout and form of spaces and the public and private interface.</i>  |
| DHC1.8.1          | Proposals should maintain or reinforce formal features such as mature trees, avenues of trees, woods and walls (as identified in the Code Analysis).   |
| 1.9               | <i>A contextual analysis should identify built character: the scale, form and massing of the built environment, treatment of building frontages and boundaries, building types and materials. This should all be included in a Character Study.</i>  |
| DHC1.9.1          | Proposals should be no more than two storeys in height.  |

# 5. Design Codes

## Dispersed Historic Core

| Place and Setting |   |
|-------------------|---|
| <b>DHC1.9.2</b>   | Proposals may be of either detached or semi-detached house built form only with gabled or cross gabled roof forms and simple rectangular floor plans predominating with the exception of old barn developments, of which Manor Farm is a fine example of adapting traditional farm buildings using high-quality design that makes a positive contribution to the rural context of the area. |
| <b>DHC1.9.3</b>   | Proposals should take into account the common use of red brick, centre or gable-end, ridge mounted chimney stacks.  |
| <b>DHC1.9.4</b>   | Proposals should consider the dominance of predominantly red brick, flemish bond with glazed headers, brick feature lintels and quoins, and plain clay tile roofs in building materials and architectural features.   |
| <b>DHC1.9.5</b>   | Proposals that comprise an architectural style of the Arts and Crafts tradition will be supported, provided they are consistent with all other relevant parts of the Code.  |
| <b>DHC1.9.6</b>   | Proposals should acknowledge the pattern of buildings fronting onto Village Green at its northern boundary.   |
| <b>DHC1.9.7</b>   | Proposals should retain and enhance mature planting along front boundaries and behind boundary walls at the southern boundary of the Village Green.   |



Manor Farm Development as an excellent example of the development of traditional farm buildings.

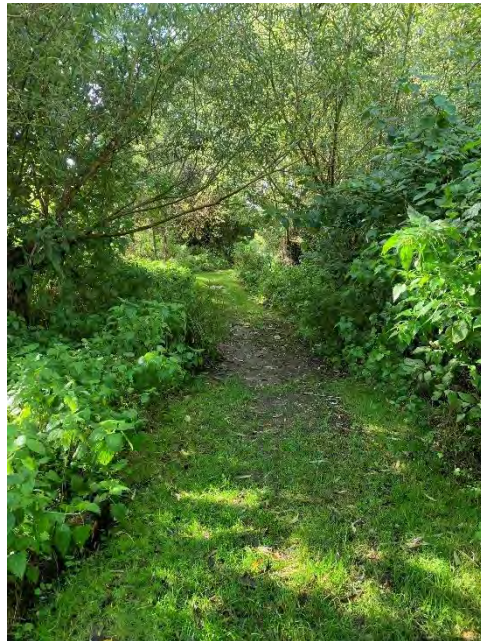
# 5. Design Codes

## Dispersed Historic Core

| Natural Environment |  |
|---------------------|--|
|                     | <i>Joint Design Guide: “The site layout should respect its physical features and those of its adjacent land including its topography, orientation, landform, geology, drainage patterns, field patterns/boundaries and vegetation cover, for example.”</i>   |
| 2.0                 | <i>retains and strengthens the site’s landscape features; using the physical features of the site and results of technical studies positively and imaginatively in its design</i>  |
| <b>DHC2.0.1</b>     | All development should contribute to the maintenance and delivery of a high quality multi-functional network of Green and Blue Infrastructure in the Parish to provide long-term benefits for people, places and nature, in ways that reinforce local character<br><br><b>See also Design Codes DHC1.0.1 &amp; DHC1.1.1</b>  |
| 2.3                 | <i>implements SuDs (Sustainable Drainage Systems) as an integral part of the development’s open space network. SuDs should be designed into the development from the outset with features such as: wetlands, basins, ponds, scrapes, swales, retention planters (rainwater gardens), combined with good landscaping to make a positive contribution to the biodiversity, character and appearance of a development</i> |
| <b>DHC2.3.1</b>     | Proposals should consider flood resistance and resilience measures such as the use of permeable paving surfaces and green, blue and brown roofs.   |
| 2.9                 | <i>trees are designed appropriately into the layout. This should be explained in the landscaping strategy</i>  |
| <b>DHC2.9.1</b>     | Proposals to fell any tree having a diameter of 9” (225mm) or more measured at 2’0” (600mm) above the ground will not be supported unless it can be demonstrated there is sufficient justification to remove the tree or it is dead, dying, dangerous or diseased.   |
| <b>DHC2.9.2</b>     | If it is necessary to remove trees to carry out a development, proposals should make provision for the replacement on a ‘one for one’ basis or where the existing tree has been identified as Significant in this Code Analysis, on a ‘two or more for one’ basis, with replacements being of a reasonable size and quality.   |
| 2.13                | <i>retains and enhances existing important habitats, creates new habitats and aims to deliver at least 10% Biodiversity Net Gain (Environment Bill 2020)</i>   |
| <b>DHC2.13.1</b>    | Proposals should embed green and blue infrastructure in ways that help support nature recovery and reverse the decline in biodiversity resulting in a ‘net gain’, including the placement of swift bricks, bat box bricks, insect bricks, house martin nest boxes, ‘hedgehog holes’ between gardens and the external natural environment avoiding openings onto roads.   |

# 5. Design Codes

## Dispersed Historic Core



# 5. Design Codes

## Dispersed Historic Core

| <b>Movement and Connectivity</b> |  |
|----------------------------------|--|
|                                  | <i>Joint Design Guide: “A place that is easy to get to and move through for all users.”</i>  |
| 3.9                              | <i>encourages movement by prioritising the needs of pedestrians, people with disabilities, cyclists and public transport users, over the needs of motorists within the design of streets. Applicants should refer to Manual for Streets 1 (2007) and 2 (2010)</i>  |
| <b>DHC3.9.1</b>                  | Proposals should ensure that any associated improvements to the highway network, where practicable, avoid urbanising highway infrastructure to preserve the rural character of the area.   |
| 3.25                             | <i>that lighting features follow the design approach used for other street furniture and avoid causing light pollution in sensitive/darker non-urban rural areas (consider, downward lighting and reduce LUX levels in these areas). Direct glare must be avoided, from any lighting scheme to neighbouring properties</i>   |
| <b>DHC3.25.1</b>                 | Proposals should avoid causing light pollution in this sensitive and dark rural area and will be expected to comply with the requirements of Policy CUL10: Light Pollution in the Culham Neighbourhood Plan. Proposals should consider the inclusion of curfew hours* as part of the lighting scheme.<br><br><i>*Curfew: The time after which stricter requirements (for the control of obtrusive light) will apply; often a condition of use of lighting applied the local planning department. Depending upon application curfew times often commence between 21:00 to 23:00 and may run until 07:00. However, exact curfew hours should be carefully applied to ensure the reduction of obtrusive light is prioritised within the immediate environment and towards sensitive human as well as fauna and flora receptors. Source: <a href="#">Institute of Lighting Professionals Guidance Note 01/21</a></i> |
| <b>Space and layout</b>          |  |
|                                  | <i>Joint Design Guide: “Use an appropriate scale and density to create a place of a human scale.”</i>  |
| 4.2                              | <i>consists of perimeter blocks that respond to the grain of the existing settlements taking cues from block sizes, plot patterns, and the relationship between built and open space</i>   |
| <b>DHC4.2.1</b>                  | The characteristic pattern of development in the character area is one where the buildings are set within the landscape; where the landscape is dominant. In this rural character area, an irregular block layout, as guided by this Design Code, is considered to provide a more appropriate ‘organic’ character and perimeter blocks will therefore be resisted.   |

# 5. Design Codes

## Dispersed Historic Core

| <b>Built Form</b>                 |   |
|-----------------------------------|---|
|                                   | <i>Joint Design Guide: “Respect the local context whilst striving for excellence in architectural quality and sustainability.”</i>  |
| 5.4                               | <i>incorporates green and/or brown roofs/roof gardens on flat roof buildings and vertical gardens. Building design should seek to integrate biodiversity enhancements wherever possible. These could be through the provision of green walls/roofs, or faunal features (bird/bat boxes). They can be discretely incorporated into structures, or made into focal points, and will contribute to the need for development to deliver biodiversity net gain</i>     |
| <b>DHC5.4.1</b>                   | Virtually any type of roof structure can accommodate green and brown roofs and should therefore not be limited for consideration on flat roofs as flat roof forms will not be appropriate in this character area. Green, brown (now also known as biodiverse roofs) and blue roofs should be explored on all roof types as a contribution to nature recovery, surface water flood alleviation and their appearance will contribute to Culham’s sense of greenery. |
| 5.25                              | <i>maintains established building lines and predominant plot patterns</i>   |
| <b>DHC5.25.1</b>                  | Proposals at properties on the northern boundary of the Village Green and on the High Street, must not lead to new buildings or existing buildings extending in front of any building line to the plot frontage that is common to both adjoining buildings.   |
| 5.59                              | <i>the proposed design must preserve or enhance the original features and/or contribute to its significance</i>   |
| <b>DHC5.59.1</b>                  | In addition, the design of proposals should enhance the original features and contribute to the significance of local heritage assets in this Code Analysis.  |
| <b>Climate and Sustainability</b> |   |
|                                   | <i>Joint Design Guide: “Achieve an optimal active design approach.”</i>   |
| 6.2                               | <i>optimises the orientation of buildings to utilise solar gain and shading</i>   |
| <b>DHC6.2.1</b>                   | All development must be ‘zero carbon ready by design’ to minimise the amount of energy needed to heat and cool buildings through landform, layout, building orientation, massing and landscaping.   |

# 5. Design Codes

## Dispersed Historic Parkland

| Place and Setting |  |
|-------------------|--|
|                   | <i>Joint Design Guide: “Ensure: A contextual analysis including an opportunities and constraints plan (which will inform your design rationale) of the wider and immediate site context has been prepared.”</i>  |
| 1.0               | <i>A contextual analysis should identify existing networks of natural features, including watercourses, trees, woodland, hedgerows, green spaces, field patterns, habitats and public rights of way (footpaths, bridleways, etc.)</i>  |
| <b>DHP1.0.1</b>   | Proposals should retain the green impression formed by Significant Trees and Hedgerows, Important Open Space (identified in this Code Analysis), fields and gardens as an important element in Culham’s character as a historic village in a rural setting and the character and appearance of the Conservation Area.  |
| <b>DHP1.0.2</b>   | Proposals should acknowledge Significant Trees and Hedgerows (identified in this Code Analysis), including ponds and priority habitats of traditional orchard and deciduous woodland as a valuable historic setting in the character and appearance of the Conservation Area contributing to the tranquillity of rural life and as a haven for wildlife.   |
| 1.1               | <i>A contextual analysis should identify the landscape character, natural features and topography highlighting visually prominent areas</i>  |
| <b>DHP1.1.1</b>   | Proposals should acknowledge, where applicable, the key characteristics of the Open Farmed Hills and Valleys, Parkland and Estate Farmland and Wooded Hills and Valleys landscape types in the Nuneham Courtenay Ridge Character Area identified in this Code.   |
| 1.2               | <i>A contextual analysis should identify attractive and/or sensitive views (both of and from built and natural features) into, out of and within the site</i>  |
| <b>DHP1.2.1</b>   | Proposals should avoid obstructing views of surviving areas of permanent pasture (identified in this Code Analysis) reflecting the rural and unspoilt character of the parkland and estate farmland landscape.   |
| <b>DHP1.2.2</b>   | Proposals should acknowledge the way in which the well-managed parkland character with formal features has been carved out from the surrounding landscape and how most plots and buildings are hidden by surrounding tree cover and vegetation with long distance views restricted by tree cover, well-established vegetation and formal boundary features, reinforcing its contribution as the backdrop to the attractive skyline of the village visible from Culham Lock Car Park and along Culham Cut (identified in this Code Analysis). |

# 5. Design Codes

## Dispersed Historic Parkland



# 5. Design Codes

## Dispersed Historic Parkland

|          | Place and Setting   |
|----------|---|
| 1.3      | <i>A contextual analysis should identify buildings and structures of historical importance including listed buildings, associated setting and historic views, historic landscape pattern and features (historic landscape character), conservation areas, historic parks and gardens and archaeological remains</i> |
| DHP1.3.1 | Proposals should protect, and where appropriate, enhance, or better reveal, the significance of the Pillbox FW3/24 on The Burycroft as a local heritage asset and an integral part of Britain's military history.   |



### Pillbox (Type FW3/24)

A hexagonal pillbox with an internal anti-ricochet wall. The rear wall was lengthened to take two rifle loopholes in addition to the five light machine gun. Both 15in and 42in thick walls are common. Designed by DFW branch 3. Built in 1940 into 1941 for the defence of the United Kingdom against a possible enemy invasion during World War II.

Source:

[https://heritagedata.org/live/schemes/eh\\_tmt2/concepts/140521.html](https://heritagedata.org/live/schemes/eh_tmt2/concepts/140521.html)



Pillbox on The Burycroft – Source

<https://www.tracesofwar.com/sights/21891/Where-is-Pillbox-FW3-22-Culham.htm>

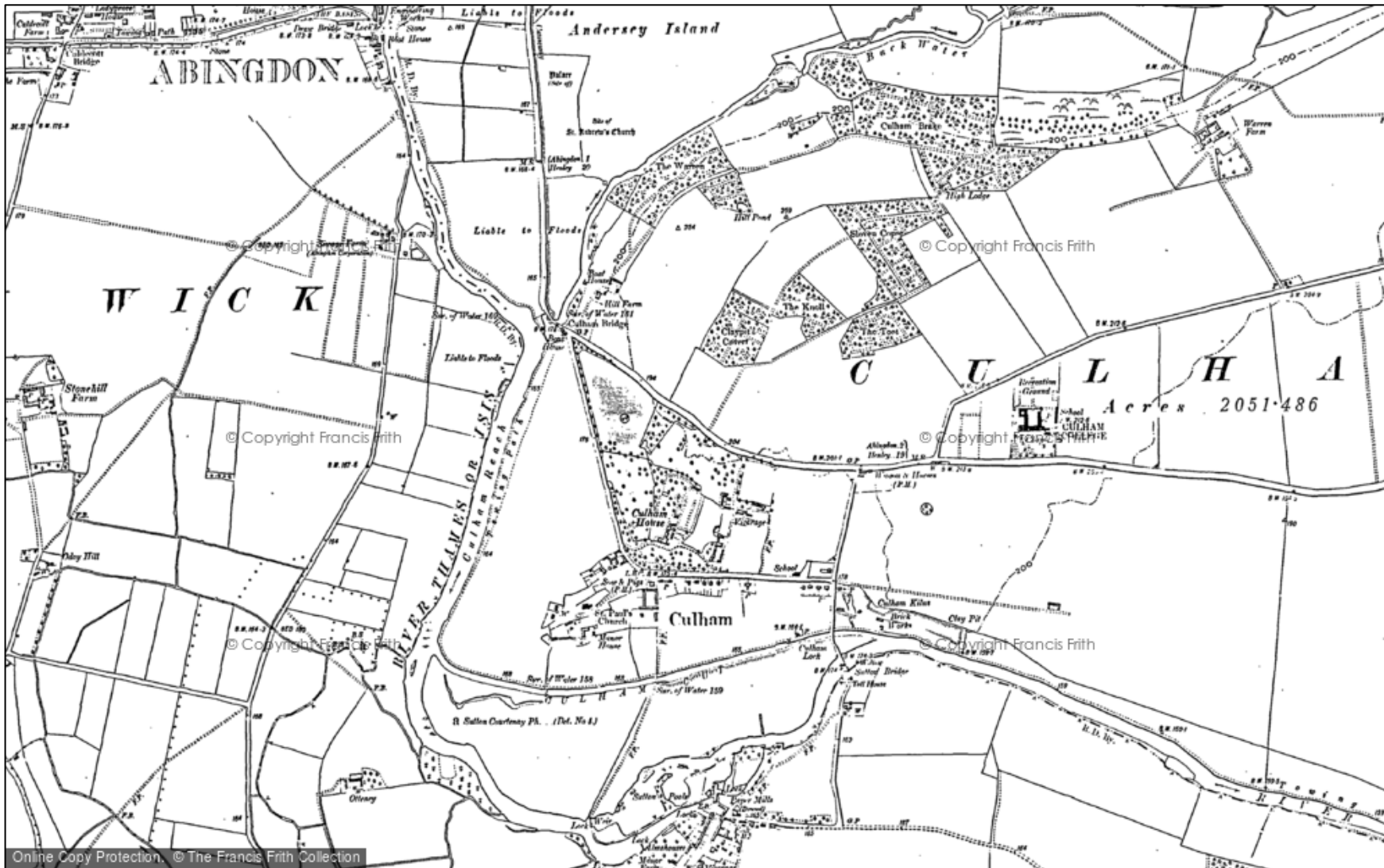
# 5. Design Codes

## Dispersed Historic Parkland

| Place and Setting |   |
|-------------------|---|
| DHP1.3.2          | <p>Proposals should acknowledge the special interest of this part of the Culham Conservation Area as highlighted in the Design Codes, including the following characteristics:</p> <ol style="list-style-type: none"> <li>The origins of the main village settlement as a typical Saxon settlement developed just above the Thames floodplain taking advantage of the higher ground and proximity to well-watered river meadows;</li> <li>The historical significance of the Grade II listed Culham House and Culham Court, both of which is less immediately visible from the road, contribute to the character and appearance of the Conservation Area;</li> <li>Important Open Spaces include the grounds of Culham House in addition to the surviving areas of permanent pasture and Culham Recreation Ground identified in this Code Analysis;</li> <li>Fine groups of mature trees, avenues of trees, lakes, woods and walls contribute to the generally enclosed character of this part of the Conservation Area. Some of the main groupings of historic trees remain to this day, indicating their planned and purposeful planting more than a century ago;</li> <li>The rural and unspoilt character of this part of the Conservation Area is enhanced by the presence of woodland and tree cover. With the exception of the Burycroft and the High Street as the main road, there are no pavements or street lights;</li> <li>The rural setting and character of this part of the Conservation Area is in danger of being spoiled by light pollution from the proposed future growth proposals to the north of the Parish with light pollution from Abingdon and Didcot already harming the character.</li> </ol> |
| 1.6               | <p><i>A contextual analysis should settlement structure of the site and surrounding area: this includes studying the historical development of the settlement, its townscape; structure and hierarchy of streets, spaces, facilities, existing connections (including footpaths and cycle routes), gateways, nodes, density, plot and block sizes. Figure ground diagrams can help explain a settlement structure.</i></p>  |
| DHP1.6.1          | <p>Infill developments will be required to demonstrate that proposed buildings on infill plots will reinforce local distinctiveness through incorporating open spaces which are characteristic of the dispersed settlement pattern to avoid reducing the open character of the area.</p>  |
| DHP1.6.2          | <p>Proposals adjacent to the junction of Tollgate Road and the A415 must acknowledge the special prominence of this location as a gateway into the historic village.</p>  |
| 1.8               | <p><i>A contextual analysis should identify the streets and public spaces surrounding the site, the enclosure of streets and public open spaces, the layout and form of spaces and the public and private interface.</i></p>  |
| DHP1.8.1          | <p>Proposals should maintain or reinforce formal features such as mature trees, avenues of trees, woods and walls (as identified in the Code Analysis).</p>   |

# 5. Design Codes

## Dispersed Historic Parkland



# 5. Design Codes

## Dispersed Historic Parkland

|                 | <b>Place and Setting</b>  |
|-----------------|---|
| 1.9             | <i>A contextual analysis should identify built character: the scale, form and massing of the built environment, treatment of building frontages and boundaries, building types and materials. This should all be included in a Character Study.</i>   |
| <b>DHP1.9.1</b> | Proposals should be no more than two storeys in height unless there is local precedence for taller buildings in the immediate vicinity.   |
| <b>DHP1.9.2</b> | Proposals should be of a detached house built form only, unless it can be demonstrated that other built forms incorporates open space that will reinforce the local distinctiveness of the dispersed settlement pattern as per Design Code DHP2.1.7 i., with the exception of old barn developments which should respect and enhance the architectural and historic interest, and setting of the buildings minimising loss to significant historic fabric and retaining distinctive features. |
| <b>DHP1.9.3</b> | Proposals for new buildings should be centralised on the plot and/or providing distance away from boundaries with neighbouring properties to promote openness.  |
| <b>DHP1.9.4</b> | Proposals may adopt a variety of architectural styles in respect of composition of the buildings and of the appearance of its materials.  |
| <b>DHP1.9.5</b> | Proposals should retain or provide hedgerows as common traditional highway boundaries, or in the case of Culham House the high level wall adjacent to the High Street and The Burycroft with mature planting behind such boundary walls, providing enclosure and screening, occasionally allowing views through gates to parkland features.   |

# 5. Design Codes

## Dispersed Historic Parkland

| Natural Environment |  |
|---------------------|--|
|                     | <i>Joint Design Guide: “The site layout should respect its physical features and those of its adjacent land including its topography, orientation, landform, geology, drainage patterns, field patterns/boundaries and vegetation cover, for example.”</i>   |
| 2.0                 | <i>retains and strengthens the site’s landscape features; using the physical features of the site and results of technical studies positively and imaginatively in its design</i>  |
| <b>DHP2.0.1</b>     | All development should contribute to the maintenance and delivery of a high quality multi-functional network of Green and Blue Infrastructure in the Parish to provide long-term benefits for people, places and nature, in ways that reinforce local character.<br><br><b>See also Design Codes DHP1.0.1; DPH1.0.2; &amp; DPH1.1.1.</b>   |
| 2.3                 | <i>implements SuDs (Sustainable Drainage Systems) as an integral part of the development’s open space network. SuDs should be designed into the development from the outset with features such as: wetlands, basins, ponds, scrapes, swales, retention planters (rainwater gardens), combined with good landscaping to make a positive contribution to the biodiversity, character and appearance of a development</i> |
| <b>DHC2.3.1</b>     | Proposals should consider flood resistance and resilience measures such as the use of permeable paving surfaces and green, blue and brown roofs.   |
| 2.9                 | <i>trees are designed appropriately into the layout. This should be explained in the landscaping strategy</i>  |
| <b>DHP2.9.1</b>     | Proposals to fell any tree having a diameter of 9” (225mm) or more measured at 2’0” (600mm) above the ground will not be supported unless it can be demonstrated there is sufficient justification to remove the tree or it is dead, dying, dangerous or diseased.   |
| <b>DHP2.9.2</b>     | If it is necessary to remove trees to carry out a development, proposals should make provision for the replacement on a ‘one for one’ basis or where the existing tree has been identified as Significant in this Code Analysis, on a ‘two or more for one’ basis, with replacements being of a reasonable size and quality.   |
| 2.13                | <i>retains and enhances existing important habitats, creates new habitats and aims to deliver at least 10% Biodiversity Net Gain (Environment Bill 2020)</i>   |
| <b>DHP2.13.1</b>    | Proposals should embed green and blue infrastructure in ways that help support nature recovery and reverse the decline in biodiversity resulting in a ‘net gain’, including the placement of swift bricks, bat box bricks, insect bricks, house martin nest boxes, ‘hedgehog holes’ between gardens and the external natural environment avoiding openings onto roads.   |

# 5. Design Codes

## Dispersed Historic Parkland

| Movement and Connectivity |  |
|---------------------------|--|
|                           | <i>Joint Design Guide: “A place that is easy to get to and move through for all users.”</i>  |
| 3.9                       | <i>encourages movement by prioritising the needs of pedestrians, people with disabilities, cyclists and public transport users, over the needs of motorists within the design of streets. Applicants should refer to Manual for Streets 1 (2007) and 2 (2010)</i>  |
| <b>DHP3.9.1</b>           | Proposals should acknowledge and respond to the need to enhance pedestrian and cycle connectivity across the A415 Abingdon Road from the existing village settlement and the prominent role the area around the junction of Tollgate Road and the A415 will play in linking together the existing settlement with new development to the north of the A415 whilst retaining the distinct separate identity of the historic rural village.  |
| 3.25                      | <i>that lighting features follow the design approach used for other street furniture and avoid causing light pollution in sensitive/darker non-urban rural areas (consider, downward lighting and reduce LUX levels in these areas). Direct glare must be avoided, from any lighting scheme to neighbouring properties</i>   |
| <b>DHP3.25.1</b>          | Proposals should avoid causing light pollution in this sensitive and dark rural area and will be expected to comply with the requirements of Policy CUL10: Light Pollution in the Culham Neighbourhood Plan. Proposals should consider the inclusion of curfew hours* as part of the lighting scheme.<br><br><i>*Curfew: The time after which stricter requirements (for the control of obtrusive light) will apply; often a condition of use of lighting applied the local planning department. Depending upon application curfew times often commence between 21:00 to 23:00 and may run until 07:00. However, exact curfew hours should be carefully applied to ensure the reduction of obtrusive light is prioritised within the immediate environment and towards sensitive human as well as fauna and flora receptors. Source: <a href="#">Institute of Lighting Professionals Guidance Note 01/21</a></i> |

# 5. Design Codes

## Dispersed Historic Parkland

| Space and layout |   |
|------------------|---|
|                  | <i>Joint Design Guide: "Use an appropriate scale and density to create a place of a human scale."</i>   |
| 4.2              | <i>consists of perimeter blocks that respond to the grain of the existing settlements taking cues from block sizes, plot patterns, and the relationship between built and open space</i>  |
| <b>DHP4.2.1</b>  | The characteristic pattern of development in the character area is one where the buildings are set within the landscape; where the landscape is dominant. In this rural character area, an irregular block layout, as guided by this Design Code, is considered to provide a more appropriate 'organic' character and perimeter blocks will therefore be resisted.  |
| Built Form       |   |
|                  | <i>Joint Design Guide: "Respect the local context whilst striving for excellence in architectural quality and sustainability."</i>  |
| 5.4              | <i>incorporates green and/or brown roofs/roof gardens on flat roof buildings and vertical gardens. Building design should seek to integrate biodiversity enhancements wherever possible. These could be through the provision of green walls/roofs, or faunal features (bird/bat boxes). They can be discretely incorporated into structures, or made into focal points, and will contribute to the need for development to deliver biodiversity net gain</i> |
| <b>DHP5.4.1</b>  | Virtually any type of roof structure can accommodate green and brown roofs and should therefore not be limited for consideration on flat roofs. Green, brown (now also known as biodiverse roofs) and blue roofs should be explored on all roof types as a contribution to nature recovery, surface water flood alleviation and their appearance will contribute to Culham's sense of greenery.   |
| 5.59             | <i>the proposed design must preserve or enhance the original features and/or contribute to its significance</i>   |
| <b>DHP5.59.1</b> | In addition, the design of proposals should enhance the original features and contribute to the significance of local heritage assets in this Code Analysis.  |

# 5. Design Codes

## Dispersed Historic Parkland

| Climate and Sustainability |   |
|----------------------------|---|
|                            | <i>Joint Design Guide: "Achieve an optimal active design approach."</i>   |
| 6.2                        | <i>optimises the orientation of buildings to utilise solar gain and shading</i>   |
| <b>DHP6.2.1</b>            | All development must be 'zero carbon ready by design' to minimise the amount of energy needed to heat and cool buildings through landform, layout, building orientation, massing and landscaping. |

# 5. Design Codes

## Linear Extensions

| Place and Setting |   |
|-------------------|---|
|                   | <i>Joint Design Guide: “Ensure: A contextual analysis including an opportunities and constraints plan (which will inform your design rationale) of the wider and immediate site context has been prepared.”</i>   |
| 1.0               | <i>A contextual analysis should identify existing networks of natural features, including watercourses, trees, woodland, hedgerows, green spaces, field patterns, habitats and public rights of way (footpaths, bridleways, etc.)</i>   |
| <b>LE1.0.1</b>    | Proposals should retain and enhance domestic front gardens, grass verges (particularly on the High Street and at The Glebe identified as Important Open Space in this Code Analysis), trees and hedgerows (particularly Significant Trees and Hedgerows identified in this Code Analysis) as an important contribution to the rural and open character of the village and as an opportunity for additional habitat provision. |
| 1.1               | <i>A contextual analysis should identify the landscape character, natural features and topography highlighting visually prominent areas</i>   |
| <b>LE1.1.1</b>    | Proposals should acknowledge, where applicable, the key characteristics of the Flat Floodplain Pasture and Open Farmed Hills and Valleys landscape types in the Nuneham Courtenay Ridge Character Area identified in this Code Analysis.  |
| 1.2               | <i>A contextual analysis should identify attractive and/or sensitive views (both of and from built and natural features) into, out of and within the site</i>   |
| <b>LE1.2.1</b>    | Proposals should acknowledge the way in which most plots and buildings are hidden by surrounding tree cover with long distance views restricted by tree cover in most parts, reinforcing the attractive skyline of this character area visible from Culham Lock Car Park and along Culham Cut (identified in this Code Analysis).   |
| <b>LE1.2.2</b>    | Proposals on the northern stretch of Tollgate Road should avoid obstructing the dominant sky and long views (identified in this Code Analysis) of the open and exposed character of the landscape to the east of the village.   |

# 5. Design Codes

## Linear Extensions

|         | Place and Setting   |
|---------|---|
| 1.3     | <i>A contextual analysis should identify buildings and structures of historical importance including listed buildings, associated setting and historic views, historic landscape pattern and features (historic landscape character), conservation areas, historic parks and gardens and archaeological remains</i> |
| LE1.3.1 | Proposals should retain the built form and architectural features of nos. 22-23 High Street as local heritage assets and should acknowledge the role the buildings play as a positive contribution to the street scene.   |
| LE1.3.2 | Proposals should retain the built form and architectural features of nos. 22-23 High Street as local heritage assets and should acknowledge the prominent location and role the buildings play as a positive contribution to the street scene.  |



### School House

Erected in 1850 with some additions made in 1897. It was reorganised in 1924 for infants and girls only, but in 1931 operated for infants only. The school temporarily closed in 1948, but was re-opened in 1951 and a single storey extension was added to the existing school building in 1994. The architectural features on the original building has been maintained and makes a positive contribution to the street scene.

### 22-23 High Street

A pair of symmetrical, semi-detached houses constructed some time between 1883 and 1899. The buildings relate to the Morrell's Brewery associated with the village through the later 19<sup>th</sup> century and early 20<sup>th</sup> century and bares a striking resemblance with the former PH The Lion further along the High Street. The buildings are constructed of high quality materials possessing visual detail and interest which contributes positively to the street scene and Culham's skyline where glimpses of the rear elevation of the buildings are visible along the Thames Path on Culham Cut.



# 5. Design Codes

## Linear Extensions

| Place and Setting |   |
|-------------------|---|
| 1.3               | <i>A contextual analysis should identify buildings and structures of historical importance including listed buildings, associated setting and historic views, historic landscape pattern and features (historic landscape character), conservation areas, historic parks and gardens and archaeological remains</i>   |
| <b>LE1.3.3</b>    | Proposals should retain the built form and architectural features of Kiln Cottage as a local heritage asset and the view of the building should not be obstructed any further.  |
| <b>LE1.3.4</b>    | Proposals should acknowledge the special interest of this part of the Culham Conservation Area as highlighted in the Design Codes, including the following characteristics: <ol style="list-style-type: none"> <li>The origins of the main village settlement as a typical Saxon settlement developed just above the Thames floodplain taking advantage of the higher ground and proximity to well-watered river meadows;</li> <li>The architectural features of the Grade II listed nos. 36 and 27 High Street contributing to the significance of the Conservation Area;</li> <li>The rural setting and character of this part of the Conservation Area is in danger of being spoiled by light pollution from the adjoining larger centres of Abingdon and Didcot and proposed future growth proposals to the north of the Parish.</li> </ol> |

### Kiln Cottage

Associated with the Culham Brick and Lime Works which started operating in about 1850 closing around 1932. The Mouldey family rented Kiln Cottage from the Morrell family and describe Kiln Cottage and the Brickworks as “a package”. Source: [Janet Brandon My Childhood in Culham](#)

A detached two storey brick building situated on a generous plot on Tollgate Road. The building is sited perpendicular to Tollgate Road with its principal elevation to the south. The installation of a boundary wall with inn 2012 now largely screens the attractive setting of the cottage.

Photo 1 B – View opposite existing vehicle entrance B toward property



Kiln Cottage prior to the erection of a boundary wall, gates and fencing in 2012 Source: [P12/S2231/HH](#)

# 5. Design Codes

## Linear Extensions

|                | <b>Place and Setting</b>  |
|----------------|---|
| 1.6            | <i>A contextual analysis should settlement structure of the site and surrounding area: this includes studying the historical development of the settlement, its townscape; structure and hierarchy of streets, spaces, facilities, existing connections (including footpaths and cycle routes), gateways, nodes, density, plot and block sizes. Figure ground diagrams can help explain a settlement structure.</i> |
| <b>LE1.6.1</b> | Proposals for plot sub-division or for more than one dwelling or change of use within in established plot will not be supported.  |
| <b>LE1.6.2</b> | Proposals should acknowledge the very regular pattern of plot shapes and sizes along every road in this area.   |
| 1.8            | <i>A contextual analysis should identify the streets and public spaces surrounding the site, the enclosure of streets and public open spaces, the layout and form of spaces and the public and private interface.</i>   |
| <b>LE1.8.1</b> | Proposals should retain and enhance the rural and open character of the village created by a combination of front gardens, grass verges and mature trees.   |
| 1.9            | <i>A contextual analysis should identify built character: the scale, form and massing of the built environment, treatment of building frontages and boundaries, building types and materials. This should all be included in a Character Study.</i>   |
| <b>LE1.9.1</b> | Proposals should be no more than two storeys in height.   |
| <b>LE1.9.2</b> | Proposals on the High Street and The Glebe may be either of a detached, semi-detached house or bungalow built form only with gabled or cross gabled roof forms and simple rectangular floor plans predominating.  |
| <b>LE1.9.3</b> | Proposals on Tollgate Road should be either of a detached, semi-detached house or bungalow built form only comprising either an open gable, cross gable, former, hipped, cross-hipped, pyramid hipped or intersecting/overlaid hipped roof form – other roof forms have no precedent in the character area.   |
| <b>LE1.9.4</b> | Proposals should take into account the common use of red brick, centre or gable-end, ridge mounted chimney stacks.  |
| <b>LE1.9.5</b> | Proposals should consider the dominance of predominantly red brick, flemish bond with glazed headers, brick feature lintels and quoins, and plain clay tile roofs in building materials and architectural features.   |
| <b>LE1.9.6</b> | Proposals that comprise an architectural style of the Arts and Crafts tradition will be supported, provided they are consistent with all other relevant parts of the Code.  |

# 5. Design Codes

## Linear Extensions

|                | <b>Place and Setting (cont)</b>   |
|----------------|---|
| <b>LE1.9.7</b> | Proposals should acknowledge the very regular patterns of plot orientation and adhere to the strong building lines of every road in this area.  |
| <b>LE1.9.8</b> | Proposals on the High Street and The Glebe should maintain and reinforce wide grass verges and front gardens with low level boundary treatments creating a spacious open character.   |
| <b>LE1.9.9</b> | Proposals on Tollgate Road should retain and provide mature hedgerows and planting as soft boundary treatments and avoid the introduction of suburban features such as hardstanding in front gardens and higher level hard boundary treatments. |

# 5. Design Codes

## Linear Extensions

| Natural Environment |  |
|---------------------|--|
|                     | <i>Joint Design Guide: “The site layout should respect its physical features and those of its adjacent land including its topography, orientation, landform, geology, drainage patterns, field patterns/boundaries and vegetation cover, for example.”</i>   |
| 2.0                 | <i>retains and strengthens the site’s landscape features; using the physical features of the site and results of technical studies positively and imaginatively in its design</i>  |
| <b>LE2.0.1</b>      | All development should contribute to the maintenance and delivery of a high quality multi-functional network of Green and Blue Infrastructure in the Parish to provide long-term benefits for people, places and nature, in ways that reinforce local character.<br><br><b>See also Design Codes LE1.0.1 &amp; LE1.1.1.</b>  |
| 2.3                 | <i>implements SuDs (Sustainable Drainage Systems) as an integral part of the development’s open space network. SuDs should be designed into the development from the outset with features such as: wetlands, basins, ponds, scrapes, swales, retention planters (rainwater gardens), combined with good landscaping to make a positive contribution to the biodiversity, character and appearance of a development</i> |
| <b>LE2.3.1</b>      | Proposals should consider flood resistance and resilience measures such as the use of permeable paving surfaces and green, blue and brown roofs.   |
| 2.9                 | <i>trees are designed appropriately into the layout. This should be explained in the landscaping strategy</i>  |
| <b>LE2.9.1</b>      | Proposals to fell any tree having a diameter of 9” (225mm) or more measured at 2’0” (600mm) above the ground will not be supported unless it can be demonstrated there is sufficient justification to remove the tree or it is dead, dying, dangerous or diseased.   |
| <b>LE2.9.2</b>      | If it is necessary to remove trees to carry out a development, proposals should make provision for the replacement on a ‘one for one’ basis or where the existing tree has been identified as Significant in this Code Analysis, on a ‘two or more for one’ basis, with replacements being of a reasonable size and quality.   |
| 2.13                | <i>retains and enhances existing important habitats, creates new habitats and aims to deliver at least 10% Biodiversity Net Gain (Environment Bill 2020)</i>   |
| <b>LE2.13.1</b>     | Proposals should embed green and blue infrastructure in ways that help support nature recovery and reverse the decline in biodiversity resulting in a ‘net gain’, including the placement of swift bricks, bat box bricks, insect bricks, house martin nest boxes, ‘hedgehog holes’ between gardens and the external natural environment avoiding openings onto roads.   |

# 5. Design Codes

## Linear Extensions

| Movement and Connectivity |  |
|---------------------------|--|
|                           | <i>Joint Design Guide: "A place that is easy to get to and move through for all users."</i>  |
| 3.9                       | <i>encourages movement by prioritising the needs of pedestrians, people with disabilities, cyclists and public transport users, over the needs of motorists within the design of streets. Applicants should refer to Manual for Streets 1 (2007) and 2 (2010)</i>  |
| <b>LE3.9.1</b>            | Proposals should ensure that any associated improvements to the highway network, where practicable, avoid urbanising highway infrastructure to preserve the rural character of the area.   |
| 3.25                      | <i>that lighting features follow the design approach used for other street furniture and avoid causing light pollution in sensitive/darker non-urban rural areas (consider, downward lighting and reduce LUX levels in these areas). Direct glare must be avoided, from any lighting scheme to neighbouring properties</i>   |
| <b>LE3.25.1</b>           | Proposals should avoid causing light pollution in this sensitive and dark rural area and will be expected to comply with the requirements of Policy CUL10: Light Pollution in the Culham Neighbourhood Plan. Proposals should consider the inclusion of curfew hours* as part of the lighting scheme.<br><br>*Curfew: <i>The time after which stricter requirements (for the control of obtrusive light) will apply; often a condition of use of lighting applied the local planning department. Depending upon application curfew times often commence between 21:00 to 23:00 and may run until 07:00. However, exact curfew hours should be carefully applied to ensure the reduction of obtrusive light is prioritised within the immediate environment and towards sensitive human as well as fauna and flora receptors. Source: <a href="#">Institute of Lighting Professionals Guidance Note 01/21</a></i> |

# 5. Design Codes

## Linear Extensions



# 5. Design Codes

## Linear Extensions

|                |   |
|----------------|---|
|                | <b>Space and layout</b>   |
|                | <i>Joint Design Guide: “Use an appropriate scale and density to create a place of a human scale.”</i>   |
| 4.2            | <i>consists of perimeter blocks that respond to the grain of the existing settlements taking cues from block sizes, plot patterns, and the relationship between built and open space</i>  |
| <b>LE4.2.1</b> | The characteristic pattern of development in the character area is one where the buildings are set within the landscape; where the landscape is dominant. In this rural character area, an irregular block layout, as guided by this Design Code, is considered to provide a more appropriate ‘organic’ character and perimeter blocks will therefore be resisted.  |
|                | <b>Built Form</b>   |
|                | <i>Joint Design Guide: “Respect the local context whilst striving for excellence in architectural quality and sustainability.”</i>  |
| 5.4            | <i>incorporates green and/or brown roofs/roof gardens on flat roof buildings and vertical gardens. Building design should seek to integrate biodiversity enhancements wherever possible. These could be through the provision of green walls/roofs, or faunal features (bird/bat boxes). They can be discretely incorporated into structures, or made into focal points, and will contribute to the need for development to deliver biodiversity net gain</i>     |
| <b>LE5.4.1</b> | Virtually any type of roof structure can accommodate green and brown roofs and should therefore not be limited for consideration on flat roofs as flat roof forms will not be appropriate in this character area. Green, brown (now also known as biodiverse roofs) and blue roofs should be explored on all roof types as a contribution to nature recovery, surface water flood alleviation and their appearance will contribute to Culham’s sense of greenery. |



Examples of Green and Brown (Biodiverse) Roofs on pitched slopes

# 5. Design Codes

## Linear Extensions

| <b>Built Form</b>                 |   |
|-----------------------------------|---|
| 5.25                              | <i>maintains established building lines and predominant plot patterns</i>   |
| <b>LE5.25.1</b>                   | Proposals must not lead to new buildings or existing buildings extending in front of any building line to the plot frontage that is common to both adjoining buildings.                           |
| 5.59                              | <i>the proposed design must preserve or enhance the original features and/or contribute to its significance</i>   |
| <b>LE5.59.1</b>                   | In addition, the design of proposals should enhance the original features and contribute to the significance of local heritage assets in this Code Analysis.                                      |
| <b>Climate and Sustainability</b> |   |
|                                   | <i>Joint Design Guide: "Achieve an optimal active design approach."</i>   |
| 6.2                               | <i>optimises the orientation of buildings to utilise solar gain and shading</i>   |
| <b>LE6.2.1</b>                    | All development must be 'zero carbon ready by design' to minimise the amount of energy needed to heat and cool buildings through landform, layout, building orientation, massing and landscaping. |

# 5. Design Codes

## Outside the main village settlement

| Place and Setting |  |
|-------------------|--|
|                   | <i>Joint Design Guide: “Ensure: A contextual analysis including an opportunities and constraints plan (which will inform your design rationale) of the wider and immediate site context has been prepared.”</i>  |
| 1.0               | <i>A contextual analysis should identify existing networks of natural features, including watercourses, trees, woodland, hedgerows, green spaces, field patterns, habitats and public rights of way (footpaths, bridleways, etc.)</i>  |
| <b>OVS1.0.1</b>   | Proposals on Thame Lane should acknowledge the dominant sky and long views (identified in this Code Analysis) of the open and exposed character of the landscape, in particular maintaining key views to the important landmarks of Wittenham Clumps and the colling towers and chimney at Didcot Power Station.   |
| 1.3               | <i>A contextual analysis should identify buildings and structures of historical importance including listed buildings, associated setting and historic views, historic landscape pattern and features (historic landscape character), conservation areas, historic parks and gardens and archaeological remains</i>  |
| <b>OVS1.3.1</b>   | Proposals should respect the setting of the Grade II listed “Schola Europea” a neo-Gothic style building erected in 1852 and designed by Joseph Clarke, a minor architect of the Victorian era.  |
| <b>OVS1.3.2</b>   | Proposals should respect the historical functional relationship and preserve and enhance the setting of the Grade II* listed Culham Station Ticket Office and Waiting Room, the Grade II listed Culham Station Overbridge, and that of Station House and The Railway Inn as local heritage assets.   |
| <b>OCS1.3.3</b>   | Proposals should retain the built form and architectural features of Tollgate Cottage and 60 Abingdon Road as local heritage assets and their role in marking the historic significance of the Dorchester turnpike road.   |
| <b>OCS1.3.4</b>   | Proposals should retain the built form and architectural features of Maud Hales Terrace as local heritage assets and should acknowledge the prominent role the buildings play in the setting of the Grade II listed and Scheduled Ancient Monument of Maud Hales Bridge.   |
| <b>OCS1.3.5</b>   | Proposals should limit the impact of light pollution on the character of the Culham Conservation Area in accordance with Design Code OVS2.5.34 i.  |
| <b>OCS1.3.6</b>   | Proposals should protect, and where appropriate, enhance, or better reveal, the significance of the Pillboxes; Pillbox Type FW3/24C (Appleford Bridge); Pillbox Type FW3/28A (Appleford Bridge); Pillbox Type FW3/28A (Sutton Bridge); Type FW3/24C (Sutton Pools); Type FW3/28A (Zouch Farm); Type FW3/28A (Tollgate Road) as local heritage assets and an integral part of Britain's military history. |

# 5. Design Codes

## Outside the main village settlement – Local Heritage Assets

### Station House

Built in 1898 and often referred to as the Station Master’s House. Some believe it was designed by Brunel, but there is no real evidence to support this. The house was once separated from the station yard by a large wooden double gate, but all that remains of this is an old, substantial fence post hidden in the hedge opposite the ticket office.

Charles Lewis is thought to be the first resident of the property. The 1901 census records the then Station Master, George William Townsend and his wife Louise, as boarding with Charles Lewis and family at ‘Station House’.



### The Railway Inn

Opening as The Railway Hotel in 1846, operating as the Jolly Porter for a short period in the past, and now the Railway Inn, the property has had a long and close association with Culham Station which opened just two years earlier. The Railway Hotel was regularly used for inquests in the 19<sup>th</sup> century and has operated as a licensed premises throughout its history, with the exception of about four years.



The Grade II\* listed Culham Station Ticket Office and Waiting Room, the Grade II listed Culham Station Overbridge, and local heritage assets Station House and The Railway Inn and their setting.

# 5. Design Codes

## Outside the main village settlement – Local Heritage Assets

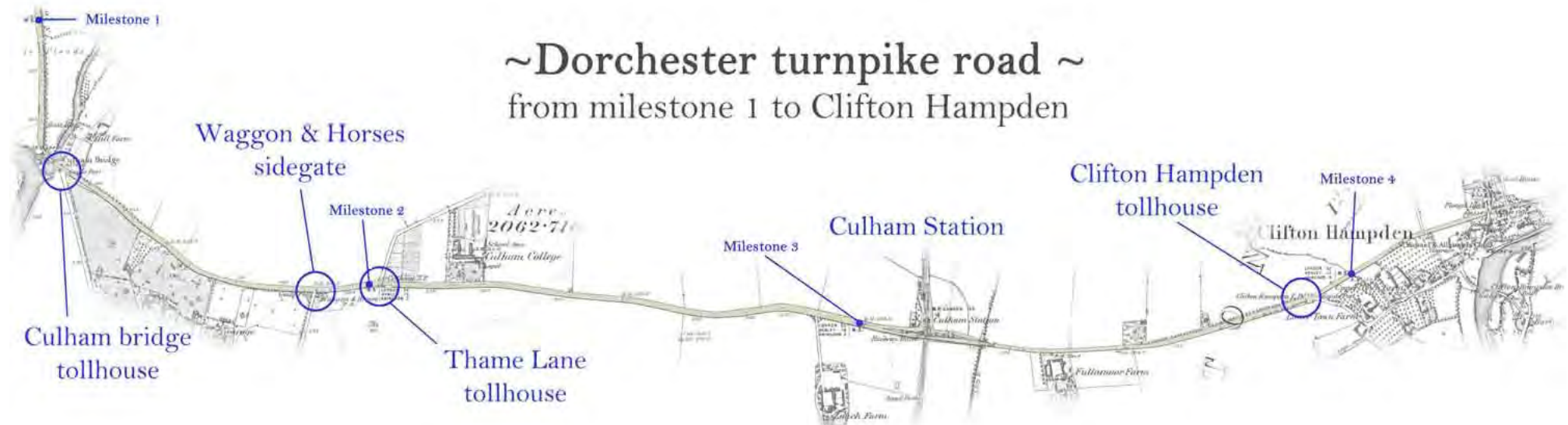
### Tollgate Cottage

A former toll house at Culham Bridge, now the private residence Tollgate Cottage. The principal structure is thought to have been built for the Henley and Dorchester Turnpike Trust in 1809 in the form of a small vernacular cottage with a front porch. There are a number of additions and alterations to the original tollhouse structure, however the eastern end of the property retains original remains. The property was sold to the Morrell family estate in 1844/45 when it ceased to be a tollhouse. The brick tollhouse was probably erected here when Sutton Courtenay Bridge was built in 1809; the old foundations of a hermitage may have been incorporated into this. Source: [Old Ticket Office at Culham](#)



### 60 Abingdon Road

A former small and simple brick toll house opposite the end of Thame Lane built towards the end of 1844. Whilst the original tollgate across the turnpike road and sidegate across Thame Lane was removed in the early 1870s, the original toll house structure still stands today and is used as a private residence. Source: [Old Ticket Office at Culham](#)



# 5. Design Codes

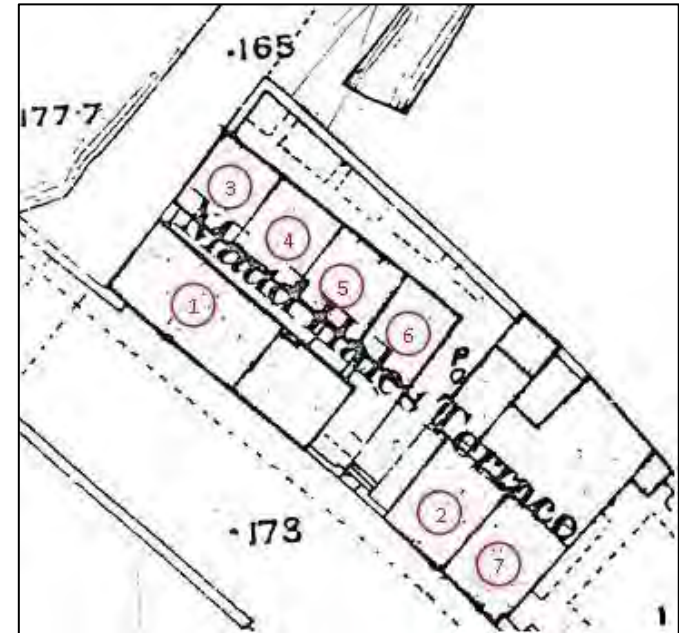
## Outside the main village settlement – Local Heritage Assets

### Maud Hales Terrace, Abingdon Bridge

*In 1429 Maud Hales, widow of William, a mercer, funded an extension of the main bridge at Abingdon – called Burford Bridge – by adding the three arches at the south end, which although rebuilt in 1929 are still known as Maud Hales’ Bridge. A stone plaque on the pair of houses at the south end – Maud Hales Terrace 1753 – is enigmatic, however. There is no evidence that the terrace had this name in the eighteenth century, and the houses on which it is placed date from the second half of the nineteenth century! The land was owned by Christ’s Hospital, whose predecessors, the Fraternity of the Holy Cross, had built the bridge in 1416, and was a garden in the 1650s when the first house – No. 1, the twin gabled cottage painted pink – was built. About a century later another house was built to the south, part of which survives behind the pair with the datestone. In the 1830s George Keates, a barge-owner, built the tall row of four narrow houses (Nos. 3-6) at the rear of the plot overlooking the meadows of Andersey Island. Although small, they had a degree of architectural flourish, some of which survives in the window dressings.*

*Later in the nineteenth century a semi-detached pair of houses (Nos. 2 and 7) was built on to the front of the southern house, which at some stage was divided and each part incorporated into the new building at the front. Christ’s Hospital sold the freehold of the entire property in 1922. In the late twentieth century the small terraced houses gained northward ground floor extensions; the owners of Nos. 2 and 7 jointly extended their houses to the rear in 1991. The first recorded lessees were carpenters and fishermen, but in the eighteenth century it was home to a number of boat-owning families, including the Gleeds and the Crawfords who were related by marriage. One Glead barge, called ‘The Abingdon’ was very large (130 tons) and had a crew of six. In the nineteenth century the rear terrace was let to labourers and workers in local factories, and outworkers including ‘slop makers’ – of cheap (sloppy) clothes.*

*Maud Hales Terrace thus survives as a testament to the prosperity of the local barge-owners and as a microcosm of local history since the mid seventeenth century.*



Annotated extract from 1<sup>st</sup> edition Ordnance Survey map



Name and date stone © D Clark 2016

Acknowledgement: This article is derived in part from notes left by the late Reverend Michael Hambleton, and the authors thank Mrs Stella Hambleton for access to them.

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# 5. Design Codes

## Outside the main village settlement – Local Heritage Assets

### Pillbox (Type FW3/24C) (Appleford Bridge)

A hexagonal pillbox with an internal anti-ricochet wall. The rear wall was lengthened to take two rifle loopholes in addition to the five light machine gun. Both 15in and 42in thick walls are common. Designed by DFW branch 3. Built in 1940 into 1941 for the defence of the United Kingdom against a possible enemy invasion during World War II.

Source:

[https://heritagedata.org/live/schemes/eh\\_tmt2/concepts/140521.html](https://heritagedata.org/live/schemes/eh_tmt2/concepts/140521.html)



Pillbox FW3/24C – Source

<https://www.tracesofwar.com/sights/22405/Pillbox-FW3-22-Culham.htm>

### Pillbox (Type FW3/28A) (Appleford Bridge)

Anti Tank Gun Emplacement. A large, rectangular pillbox based on the FW3/28 with the addition of a small infantry chamber to one side of the main gun chamber. Built in 1940 into 1941 for the defence of the United Kingdom against a possible enemy invasion during World War II.

Source:

[https://heritagedata.org/live/schemes/eh\\_tmt2/concepts/140521.html](https://heritagedata.org/live/schemes/eh_tmt2/concepts/140521.html)

# 5. Design Codes

## Outside the main village settlement – Local Heritage Assets

### Pillbox (Type FW3/28A) (Sutton Bridge)

Anti Tank Gun Emplacement. A large, rectangular pillbox based on the FW3/28 with the addition of a small infantry chamber to one side of the main gun chamber. Built in 1940 into 1941 for the defence of the United Kingdom against a possible enemy invasion during World War II.

Source:

[https://heritagedata.org/live/schemes/eh\\_tmt2/concepts/140521.html](https://heritagedata.org/live/schemes/eh_tmt2/concepts/140521.html)



Pillbox FW3/28A – Source

<https://www.tracesofwar.com/sights/22364/Pillbox-FW3-28A-Culham.htm>

### Pillbox (Type FW3/24C) (Sutton Pools)

A hexagonal pillbox with an internal anti-ricochet wall. The rear wall was lengthened to take two rifle loopholes in addition to the five light machine gun. Both 15in and 42in thick walls are common. Designed by DFW branch 3. Built in 1940 into 1941 for the defence of the United Kingdom against a possible enemy invasion during World War II.

Source:

[https://heritagedata.org/live/schemes/eh\\_tmt2/concepts/140521.html](https://heritagedata.org/live/schemes/eh_tmt2/concepts/140521.html)



Pillbox FW3/24C – Source

<https://www.tracesofwar.com/sights/22396/Pillbox-FW3-22-Culham.htm>

# 5. Design Codes

## Outside the main village settlement – Local Heritage Assets

### Pillbox (Type FW3/28A) (Zouch Farm)

Anti Tank Gun Emplacement. A large, rectangular pillbox based on the FW3/28 with the addition of a small infantry chamber to one side of the main gun chamber. Built in 1940 into 1941 for the defence of the United Kingdom against a possible enemy invasion during World War II.

Source:

[https://heritagedata.org/live/schemes/eh\\_tmt2/concepts/140521.html](https://heritagedata.org/live/schemes/eh_tmt2/concepts/140521.html)



Pillbox FW3/28A – Source

<https://www.tracesofwar.com/sights/22222/Pillbox-FW3-28A-Culham.htm>

### Pillbox (Type FW3/28A) (Tollgate Road)

Anti Tank Gun Emplacement. A large, rectangular pillbox based on the FW3/28 with the addition of a small infantry chamber to one side of the main gun chamber. Built in 1940 into 1941 for the defence of the United Kingdom against a possible enemy invasion during World War II.

Source:

[https://heritagedata.org/live/schemes/eh\\_tmt2/concepts/140521.html](https://heritagedata.org/live/schemes/eh_tmt2/concepts/140521.html)



Pillbox FW3/24C – Source

<https://www.tracesofwar.com/sights/22363/Pillbox-FW3-28A-Culham.htm>

# 5. Design Codes

## Outside the main village settlement

| The Natural Environment |  |
|-------------------------|--|
|                         | <i>Joint Design Guide: “The site layout should respect its physical features and those of its adjacent land including its topography, orientation, landform, geology, drainage patterns, field patterns/boundaries and vegetation cover, for example.”</i>   |
| 2.0                     | <i>retains and strengthens the site’s landscape features; using the physical features of the site and results of technical studies positively and imaginatively in its design</i>  |
| <b>OVS2.0.1</b>         | All development should contribute to the maintenance and delivery of a high quality multi-functional network of Green and Blue Infrastructure in the Parish to provide long-term benefits for people, places and nature, in ways that reinforce local character.   |
| 2.3                     | <i>implements SuDs (Sustainable Drainage Systems) as an integral part of the development’s open space network. SuDs should be designed into the development from the outset with features such as: wetlands, basins, ponds, scrapes, swales, retention planters (rainwater gardens), combined with good landscaping to make a positive contribution to the biodiversity, character and appearance of a development</i> |
| <b>OVS2.3.1</b>         | Proposals should consider flood resistance and resilience measures such as the use of permeable paving surfaces and green, blue and brown roofs.   |
| 2.13                    | <i>retains and enhances existing important habitats, creates new habitats and aims to deliver at least 10% Biodiversity Net Gain (Environment Bill 2020)</i>   |
| <b>OVS2.13.1</b>        | Proposals should embed green and blue infrastructure in ways that help support nature recovery and reverse the decline in biodiversity resulting in a ‘net gain’, including the placement of swift bricks, bat box bricks, insect bricks, house martin nest boxes, ‘hedgehog holes’ between gardens and the external natural environment avoiding openings onto roads.   |

# 5. Design Codes

## Outside the main village settlement



# 5. Design Codes

## Outside the main village settlement

| Movement and Connectivity |  |
|---------------------------|--|
|                           | <i>Joint Design Guide: “A place that is easy to get to and move through for all users.”</i>  |
| 3.9                       | <i>encourages movement by prioritising the needs of pedestrians, people with disabilities, cyclists and public transport users, over the needs of motorists within the design of streets. Applicants should refer to Manual for Streets 1 (2007) and 2 (2010)</i>  |
| <b>OVS3.9.1</b>           | Proposals should acknowledge and respond to the need to enhance pedestrian and cycle connectivity across the A415 Abingdon Road from the existing village settlement and the prominent role the area around the junction of Tollgate Road and the A415 will play in linking together the existing settlement with new development to the north of the A415 whilst retaining the distinct separate identity of the historic rural village. Improvements to the crossing at the junction of Tollgate Road and the A415 and improvements to the existing shared pedestrian and cycleway on the A415 will be expected to form part of the provision of sustainable transport facilities required by Policy STRAT9 of the adopted South Oxfordshire Local Plan.   |
| <b>OVS3.9.2</b>           | Proposals should acknowledge and respond to the need to enhance pedestrian and cycle connectivity alongside the railway to Oxford via Radley joining Sustrans Cycle Route 5 including a river crossing.  |
| 3.25                      | <i>that lighting features follow the design approach used for other street furniture and avoid causing light pollution in sensitive/darker non-urban rural areas (consider, downward lighting and reduce LUX levels in these areas). Direct glare must be avoided, from any lighting scheme to neighbouring properties</i>   |
| <b>OVS3.25.1</b>          | Proposals should avoid causing light pollution in this sensitive and dark rural area and will be expected to comply with the requirements of Policy CUL10: Light Pollution in the Culham Neighbourhood Plan. Proposals should consider the inclusion of curfew hours* as part of the lighting scheme.<br><br><i>*Curfew: The time after which stricter requirements (for the control of obtrusive light) will apply; often a condition of use of lighting applied the local planning department. Depending upon application curfew times often commence between 21:00 to 23:00 and may run until 07:00. However, exact curfew hours should be carefully applied to ensure the reduction of obtrusive light is prioritised within the immediate environment and towards sensitive human as well as fauna and flora receptors. Source: <a href="#">Institute of Lighting Professionals Guidance Note 01/21</a></i> |

# 5. Design Codes

## Outside the main village settlement

| Built Form                 |   |
|----------------------------|---|
|                            | <i>Joint Design Guide: “Respect the local context whilst striving for excellence in architectural quality and sustainability.”</i>  |
| 5.4                        | <i>incorporates green and/or brown roofs/roof gardens on flat roof buildings and vertical gardens. Building design should seek to integrate biodiversity enhancements wherever possible. These could be through the provision of green walls/roofs, or faunal features (bird/bat boxes). They can be discretely incorporated into structures, or made into focal points, and will contribute to the need for development to deliver biodiversity net gain</i> |
| <b>OVS5.4.1</b>            | Virtually any type of roof structure can accommodate green and brown roofs and should therefore not be limited for consideration on flat roofs. Green, brown (now also known as biodiverse roofs) and blue roofs should be explored on all roof types as a contribution to nature recovery, surface water flood alleviation and their appearance will contribute to Culham’s sense of greenery.   |
| Climate and Sustainability |   |
|                            | <i>Joint Design Guide: “Achieve an optimal active design approach.”</i>   |
| 6.2                        | <i>optimises the orientation of buildings to utilise solar gain and shading</i>   |
| <b>OVS6.2.1</b>            | All development must be ‘zero carbon ready by design’ to minimise the amount of energy needed to heat and cool buildings through landform, layout, building orientation, massing and landscaping.   |

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## APPENDIX C – ZERO CARBON BUILDINGS

1. The UK Parliament declared an environment and climate emergency<sup>1</sup> in May 2019, after South Oxfordshire District Council which did so in February 2019. The Climate Change Act 2008<sup>2</sup> is the basis for the UK's approach to tackling and responding to climate change. It requires that emissions of carbon dioxide and other greenhouse gases are reduced and that climate change risks are prepared for. The Act also establishes the framework to deliver on these requirements and commits the UK government by law to reducing greenhouse gas emissions to 'net zero' by 2050.

2. SODC Local Plan Policy DES10 was developed prior to Government committing the UK in law to 'net zero' by 2050 as per the Climate Change Act 2008 (as amended) and emission cuts of 78% by 2035 to bring UK Law in line with the recommendations of the Committee on Climate Change (CCC) Sixth Carbon Budget Report, and the Paris Agreement commitments. The Tyndall Centre for Climate Research Carbon Budget Tool confirms that South Oxfordshire District Council to make its fair contribution to delivering the Paris Agreement's commitment, an immediate and rapid programme of decarbonisation is needed. At 2017 CO<sub>2</sub> emission levels South Oxfordshire will exceed the recommended carbon budget available until 2050 in 7 years (by 2027).

3. The Energy White Paper published in December 2020 sets out the government's Vision and 10-point transition plan for how the UK will reach the UK target of 'net zero' carbon emissions by 2050. The White Paper confirms the government's intention to ensure significant strides are made to improve building energy performance to meet this target. This means that by 2030 all new buildings must operate at 'net zero', the means by which this can be achieved is described in the diagram overleaf.

4. Planning plays an important role in minimising our contribution to and increasing resilience to the effects of climate change. It can provide a positive and encouraging framework for change and can resist harmful development. The CCC highlights that we need to build new buildings with 'ultra-low' levels of energy use. The CCC also makes a specific reference to space heating demand and recommends a maximum of 15-20 kWh/m<sup>2</sup>/yr for new dwellings<sup>34</sup>.

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<sup>1</sup> 'Emergency' – "a sudden serious and dangerous event or situation which needs immediate action to deal with it"

<sup>2</sup> Amended by Climate Change Act 2008 (2050 Target Amendment) Order – SI 2019/1056 - 26 June 2019

<sup>3</sup> The UK housing: Fit for the future? report published by the Committee on Climate Change in February 2019 recommends ultra-low levels of energy use and a space heating demand of less than 15-20 kWh/m<sup>2</sup>/yr. ([Link](#))

<sup>4</sup> The costs and benefits of tighter standards for new buildings report, produced by Currie & Brown and AECOM for the Committee on Climate Change's UK housing: Fit for the future? Report ([Link](#))

# Net Zero Operational Carbon

## Ten key requirements for new buildings

By 2030 all new buildings must operate at net zero to meet our climate change targets. This means that by 2025 all new buildings will need to be designed to meet these targets. This page sets out the approach to operational carbon that will be necessary to deliver zero carbon buildings. For more information about any of these requirements and how to meet them, please refer to the: UKGBC - Net Zero Carbon Buildings Framework; BBP - Design for Performance initiative; RIBA - 2030 Climate Challenge; GHA - Net Zero Housing Project Map; CIBSE - Climate Action Plan; and, LETI - Climate Emergency Design Guide.

### Low energy use

- 1 Total Energy Use Intensity (EUI) - Energy use measured at the meter should be equal to or less than:
  - 35 kWh/m<sup>2</sup>/yr (GIA) for residential<sup>1</sup>

For non-domestic buildings a minimum DEC B (40) rating should be achieved and/or an EUI equal or less than:

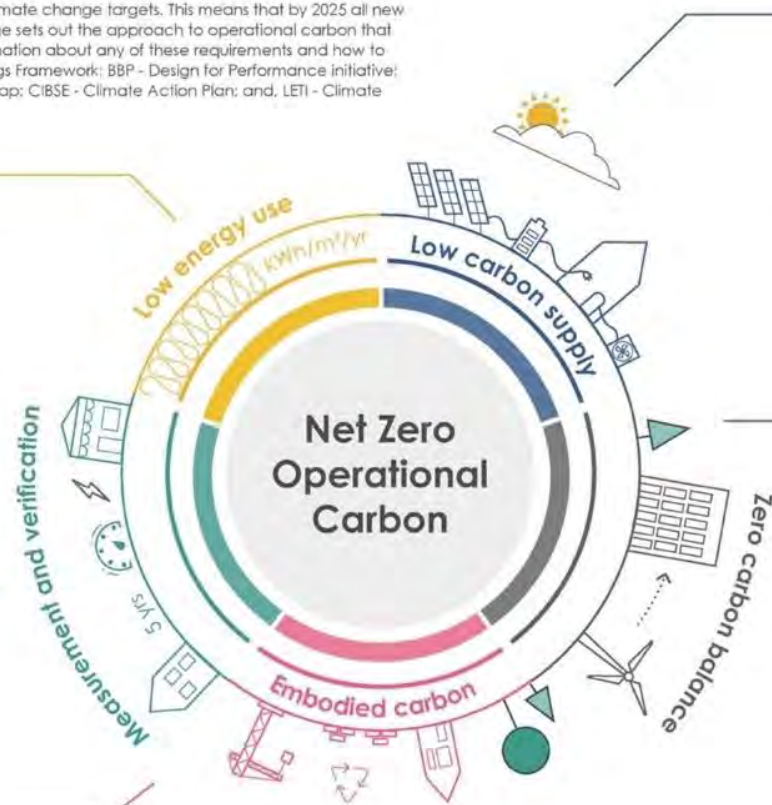
  - 65 kWh/m<sup>2</sup>/yr (GIA) for schools<sup>1</sup>
  - 70 kWh/m<sup>2</sup>/yr (NLA) or 55 kWh/m<sup>2</sup>/yr (GIA) for commercial offices<sup>1,2</sup>
- 2 Building fabric is very important therefore space heating demand should be less than 15 kWh/m<sup>2</sup>/yr for all building types.

### Measurement and verification

- 3 Annual energy use and renewable energy generation on-site must be reported and independently verified in-use each year for the first 5 years. This can be done on an aggregated and anonymised basis for residential buildings.

### Reducing construction impacts

- 4 Embodied carbon should be assessed, reduced and verified post-construction.<sup>3</sup>



### Low carbon energy supply

- 5 Heating and hot water should not be generated using fossil fuels.
- 6 The average annual carbon content of the heat supplied (gCO<sub>2</sub>/kWh) should be reported.
- 7 On-site renewable electricity should be maximised.
- 8 Energy demand response and storage measures should be incorporated and the building annual peak energy demand should be reported.

### Zero carbon balance

- 9 A carbon balance calculation (on an annual basis) should be undertaken and it should be demonstrated that the building achieves a net zero carbon balance.
- 10 Any energy use not met by on-site renewables should be met by an investment into additional renewable energy capacity off-site OR a minimum 15 year renewable energy power purchase agreement (PPA). A green tariff is not robust enough and does not provide 'additional' renewables.

#### Notes:

**Note 1 - Energy use intensity (EUI) targets**  
The above target (include all energy use) in the building (regulated and unregulated) as measured at the meter and exclude on-site generation, they have been derived from predicted energy use modelling for best practice: a review of the best performing buildings in the UK; and a preliminary assessment of the renewable energy ready for UK buildings. They are likely to be revised as more knowledge is available in these three areas. As heating and hot water is not generated by fossil fuels, this assumes an all electric building until alternative carbon fuels exist, which target are the same as kWh/m<sup>2</sup>/yr. Once other low carbon heating fuels are available this metric will be adapted.

**Note 2 - Commercial offices**  
With a typical net to gross ratio, 70 kWh/m<sup>2</sup>/yr is equivalent to 55 kWh/m<sup>2</sup>/yr GIA. Building owners and developers are recommended to target a base building rating of 6 stars using the BBP Design for Performance process based on FABRIC.

**Note 3 - Whole life carbon**  
It is recognised that operational emissions represent only one aspect of net zero carbon in new buildings. Resolving whole life carbon is crucial and will be covered in separate guidance.

**Note 4 - Adaptation to climate change**  
Net zero carbon buildings should also be adapted to climate change. It is essential that the risk of overheating is managed and that cooling is allowed.

Developed in collaboration with:



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5. A 'net zero' carbon building is therefore first and foremost an energy efficient building in which the amount of energy used for heating or cooling is minimised, as is the demand on the energy supply network.

6. This approach unequivocally focuses on the Energy Hierarchy – [BE LEAN, BE CLEAN, BE GREEN, BE SEEN](#) – the latter requiring comprehensive post occupancy monitoring, verification and rectification (if necessary) to ensure buildings perform in the way approved at design stage, ensure planning commitments are delivered and any 'performance gap' issues are resolved.

7. There is a significant weight of evidence that buildings rarely live up to their designers expectations when completed and occupied, and depart significantly from the standards against which they were certified at design stage. This is known as the 'performance gap' and is a widely acknowledged problem<sup>5</sup>. Research indicates this gap in in-use energy consumption can be anything from 2 to 5 times higher than designed for.

8. The consultation on the 'Future Buildings Standard' announced in January 2021 aims to 'radically improve' the energy performance of new homes ensuring they are 'zero carbon ready' by 2025. This means having high levels of energy efficiency and fabric performance that produce 75 to 80 per cent lower carbon emissions than houses built to current standards.

9. The consultation on the 'Future Buildings Standard' announced in January 2021 aims to 'radically improve' the energy performance of new homes ensuring they are 'zero carbon ready' by 2025. This means having high levels of energy efficiency and fabric performance that produce 75 to 80 per cent lower carbon emissions than houses built to current standards.

10. By 'Zero Carbon Ready' the Government has confirmed this means that no further retrofit work will be necessary to enable them to become zero carbon homes. To do otherwise, as the Consultation Impact Assessment (CIA) confirms, would create homes which are not fit for purpose and would pass on a significant financial liability to future homeowners, many of whom may be struggling to meet the purchase price or rental costs of their new home in the first place. It could also unnecessarily push householders into fuel poverty. A Climate Change Committee Report in 2019<sup>6</sup> confirmed the costs of achieving higher energy performance standards via retrofit can be five times the cost (about £25000 per home) compared to designing these requirements into new buildings from the outset.

11. SODC Local Plan Policy DES10 will require retrofit which will result in disturbance to future occupiers and may contribute to pushing householders into fuel poverty. A recent appeal decision notes *"It seems to me folly to build new houses now that will commit owners to potentially expensive and disruptive alterations as the UK moves to decarbonise heating of its housing stock"*. East Hampshire District Council have also confirmed that it will demand zero-carbon

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<sup>5</sup> Section 3.3. The Future Buildings Standard consultation, Jan 2021 ([Link](#))

<sup>6</sup> The Costs and Benefits of tighter standards for new buildings; Final Report for Climate Change Committee 2019 ([Link](#))

homes in its new Local Plan with the Leader of the Council echoing the Planning Inspector's position: *"It is ridiculous that homes being built now will need to be retro-fitted with energy-saving measures in 10 or 15 years' time. Today's homes should be built to meet tomorrow's challenges"*.

12. In January 2021, the Government in their response to the Future Homes Standard (FHS) consultation<sup>7</sup>, acknowledged the legislative framework had moved on since the publication of the Written Ministerial Statement (WMS) in March 2015 (HCWS488). The response confirmed that to provide certainty in the immediate term, the Government would allow local energy efficiency standards for new homes to be set locally. This is further supported by the legal opinion supplied by the Environmental Law Foundation in relation to the North Hinksey Neighbourhood Plan which confirms that the WMS from March 2015 appears to have been superseded by subsequent events and should not be read in isolation. To all intents and purposes, the WMS is now longer relevant to plan making.

13. The NPPF states at paragraph 148 that:  
*"The planning system should support the transition to a low carbon future in a changing climate...it should help to shape places in ways that contribute to radical reductions in greenhouse gas emissions..."(Plan emphasis)*

14. The NPPF also makes clear that 'landform, layout, building orientation, massing and landscaping' all contribute to well-designed places which are both efficient and resilient to climate change. The Government's Net Zero Strategy: Build Back Greener - October 2021 confirms a commitment to review the NPPF to make sure it contributes to climate change mitigation and adaptation as fully as possible.

15. There are therefore a number of ways in which climate change may be mitigated in a local area using land use and development management policies. Neighbourhood plans are well suited to providing this policy framework in the interim, where there is an absence of up to date strategic policies at the Local Plan level. Aside from ensuring sustainable patterns of land uses in settlements, policies can be used to minimise the energy demand of buildings, to store carbon and to generate renewable energy. National planning policy encourages each of them but does not specify precisely how a local area should go about realising opportunities.

16. There are practical ways that each can be delivered in a local area. The Passivhaus standard has been shown to be the most effective means of improving the energy performance of new and existing buildings. The more buildings, of all uses, that meet this standard, the better. And storing emitted carbon in plant life can reduce atmospheric carbon dioxide that is increasing global temperatures. The more that storage capacity in the local area is increased, the greater the contribution to reducing the pace of temperature increases.

17. The Government's Heat and Building's Strategy highlights the need for local, as well as national, level to achieve Net Zero and refers specifically to the 'Local

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<sup>7</sup> The Future Homes Standard : 2019 Consultation on changes to Part L (conservation of fuel and power) and Part F (ventilation) of the Building Regulations for new dwellings. Summary of response received and Government response; MHCLG. Exec Summary Page 4. ([Link](#))

Climate Action' chapter in the Net Zero Strategy. A key commitment of that Strategy being to promote best practice...and share successful net zero system solutions. Policy CUL9 is therefore intended as an interim measure until South Oxfordshire review and update their current policy.

18. Policy CUL9 will ensure the updated legal framework will apply in the Parish, and the Parish Council will willingly offer this policy to South Oxfordshire District Council to help frame a District-wide policy in the new Local Plan.

19. Furthermore, Policy CUL9 also applies the 'precautionary principle' which provides the basis to anticipate, avoid and mitigate threats to the environment. Hence, the policy acknowledges the CCC's Sixth Carbon Budget recommendation that delaying action or a failure to follow the critical dates in the 'balanced pathway'<sup>8</sup> will require costly corrective action in the future<sup>9</sup>.

20. The Government addressed the CCC's recommendation head on in their response to the Future Homes Standard consultation<sup>10</sup>. Confirming that 'it is significantly cheaper and easier to install energy efficiency and low carbon heating measures when homes are built, rather than retrofitting them afterwards'. Failure to implement Policy CUL9 on new development will add to the existing and costly retrofit burden that will be required of the existing housing stock in the Parish; only adding to the costs across South Oxfordshire as a whole.

21. In respect of the impact of Policy CUL9 on scheme viability, any extra-over cost of building to the 'zero carbon ready' Passivhaus Standard – UK evidence is indicating that this is already less than 5% and will fall to zero well within the period of this Neighbourhood Plan, as per both the Government's and CCC's impact assessments and research by the Passivhaus Trust. The policy will ensure that expensive and unnecessary retrofit costs are not passed down to building occupiers in the future, particularly in an area which has relatively high property values. Scheme viability will not therefore be acceptable as a reason for not using the Standard, unless the applicant can demonstrate the scheme has abnormal development costs to accommodate.

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<sup>8</sup> The Sixth Carbon Budget: The UK's Path to Net Zero; Committee on Climate Change, December 2020. Table 3.2a page 112. ([Link](#))

<sup>9</sup> *ibid* (vi): Paragraph 5.3 'Retrofit Costs'.

<sup>10</sup> *Ibid* (vii): Paragraph 1.4 'Net zero emissions and climate change'.

## APPENDIX D – POST OCCUPANCY EVALUATION NOTE

Pulling on latest guidance and best practice, this guidance note sets out how Post-Occupancy Evaluation (POE) should be undertaken.

1.01 Post-Occupancy Evaluation (POE) is the method of obtaining feedback on a building's energy performance 'in use', to ensure it measures up to the commitments made by the team that designed and built it. It offers significant potential to address the performance gap and occupant satisfaction.

1.02 Where a monitoring regime to ensure the 'as designed' building performance targets are achieved in practice for all new and refurbished buildings is required, it is important that data is collected robustly, following good practice POE principles. It is therefore recommended that for residential development the POE methodology in section 11.4 of the Home Quality Mark ONE: Technical Manual: England, Scotland & Wales SD239 (2018)<sup>58</sup>, or as updated, is used as a guide for meeting this requirement. For non-residential buildings the BSRIA Soft Landings and Design for Performance framework (BG 76/2019), or as updated, may be used.

1.03 Applicants are required to set out in their Energy Statement how their monitoring regime, based on the HQM, BSRIA or similar methodology, will work in practice and be independently verified by a third party. The Energy Statement to be submitted with the planning application.

1.04 As each new or refurbished building comes into use, the developer must ensure performance monitoring and data collection for all relevant parameters for one whole year is carried out once the building is substantially occupied, in line with good POE practice for residential or non-residential uses. This verification process should entail, after appropriate commissioning has taken place, comparison of the 'as designed' parameters (energy, carbon, air quality and overheating risk) to monitoring data under the same categories, to assess and compare actual performance.

1.05 In order to account for seasonality, a minimum of 12 months monitoring data is required. On the other hand, to account for actual weather, the modelling results can be adjusted with degree days for the relevant year.

1.06 A 'performance gap metric', which will compare designed and actual performance (e.g. a percentage difference) for each of the 4 required parameters (energy, carbon, air quality and overheating risk) should be issued at POE stage. This needs to be issued for both the 'central' scenario and the 'lowest acceptable performance /reasonable worst-case scenario' as a minimum, with multiple scenarios considered if at all possible.

1.07 The process and reporting methodology used for the POE will need to be repeatable, so that performance can be monitored for at least 2 annual space heating cycles.

1.08 A report will then be required to be submitted to both building owners/occupiers and to South Oxford District Council, which states the performance gap metric and identifies any reasons for deviation from predicted energy usage, carbon emissions, indoor air quality and overheating performance, as well as recommendations for reasonable corrective action that will be taken to reduce or eliminate the performance gap.

1.09 The submission of the monitoring report to owners/occupiers and the council must be secured by planning condition, to be determined at the time of application based on case-specific factors. The applicant must demonstrate that the reasonable corrective actions committed to in the monitoring report, and subsequently agreed by South Oxfordshire District Council, have been implemented through another annual heat cycle before the condition will be discharged.